

# MARTIN CITY AREA PLAN



KANSAS CITY  
MISSOURI





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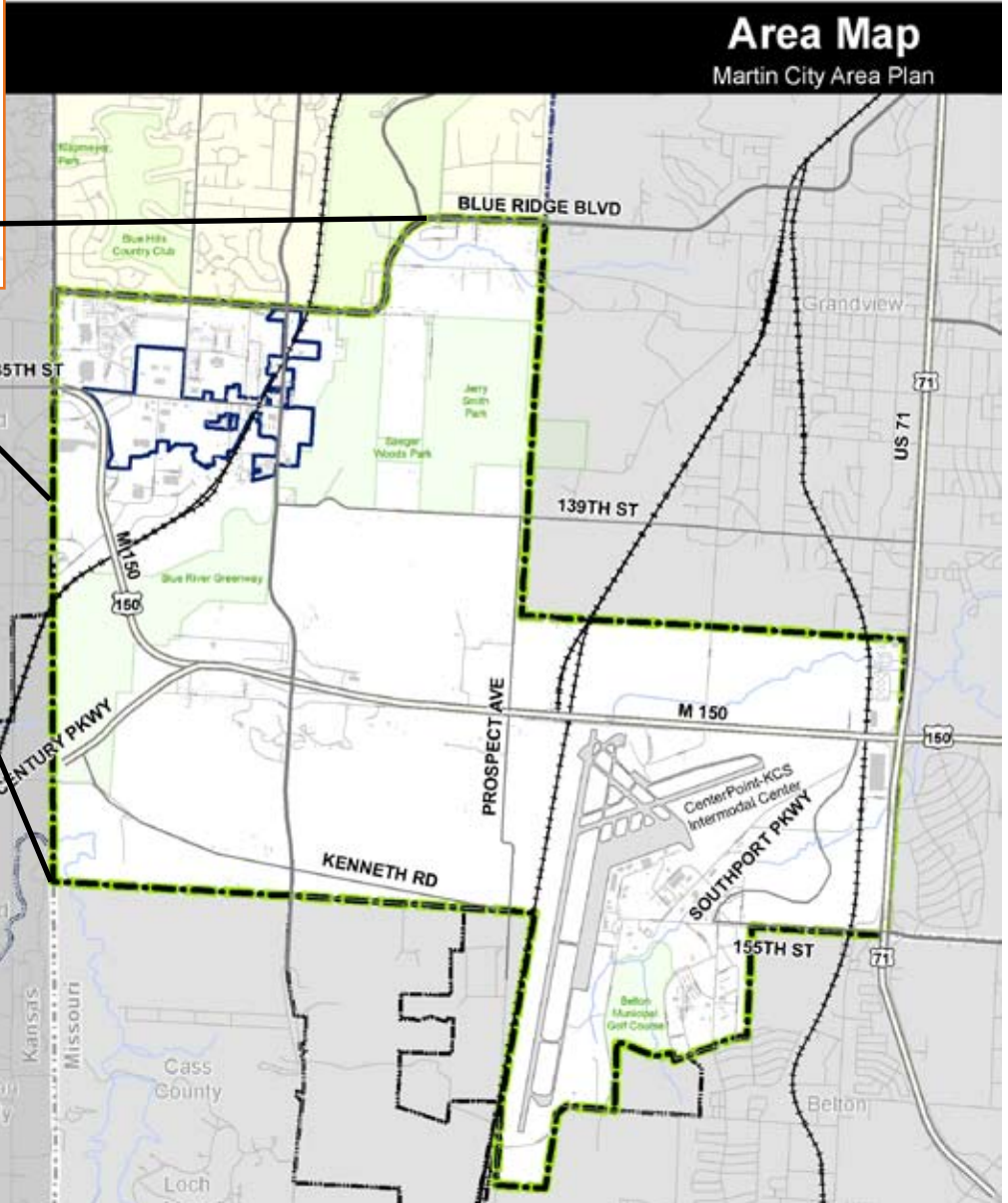
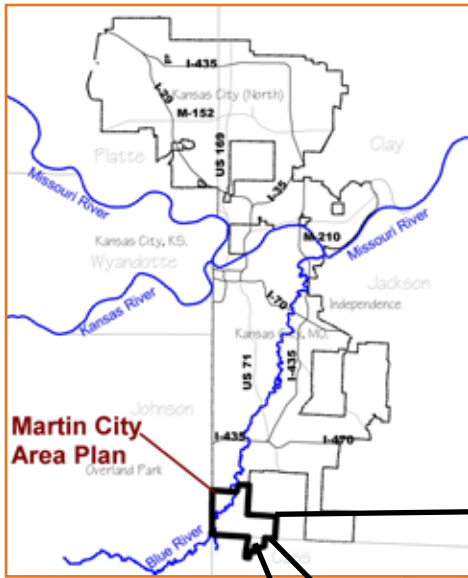
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# PLAN PURPOSE and process

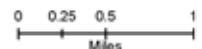
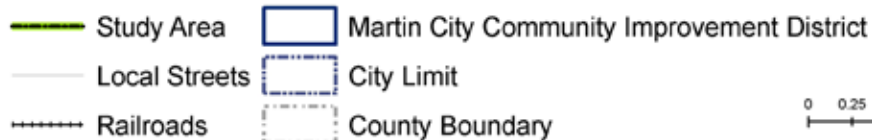
## Introduction

The Martin City Area Plan (adopted by the City Council on ??-??-2008 by Resolution Number ??????) is an update of the Martin City/Richards-Gebaur Area Plan which was adopted in May, 2001.



### GENERAL BOUNDARIES:

- North: Blue Ridge Boulevard
- South: Richard-Gebaur Airport
- East: Prospect Avenue and US Highway 71
- West: State Line Road



MCCID Boundaries as of June, 2008



The Martin City/Richards Gebaur Area Plan was adopted by the City Council in 2001. Seven years later there have been significant changes in the area, including the opening the new I50 Highway and subsequent new retail development, the establishment of the Martin City Community Improvement District (MCCID) for the Martin City area, and plans for a new intermodal facility at the former Richards Gebaur Airport. Furthermore, the CID has completed a Master Plan for the Martin City area which necessitates amendments to the area plan and a re-examination of areas surrounding the CID for compatibility and consistency. In light of these events, the City Planning and Development Department has completed an update of the Martin City/Richards Gebaur Area Plan (renamed the Martin City Area Plan). This is an update of the plan and not a complete overhaul. The 2001 plan's organizing framework and guiding principles remain unchanged. The plan has been revised and amended to incorporate recommendations of the new CID Master Plan, account for recent developments and integrate other City initiatives (i.e. Citywide Trails Plan and new Development Code).

## Plan Purpose

For a number of years, there has been both interest and concern regarding the Martin City area. The original Town of Martin City, now a quaint district of restaurants, retail, and residences, is centered at I35th Street and Holmes Road. It offers a great potential for reinforcement as a mixed-use center in southern Kansas City.

The former Richards-Gebaur Airport, has emerged as a focal point for business, industry, institutions, and area residents.

In 1999, voters in Kansas City approved the development of an intermodal freight hub at Richards-Gebaur (the Centerpoint Intermodal Facility). Since its creation, significant industrial development has occurred, and industrial development pressures continue. This has caused concerns among Kansas City residents, as well as people from neighboring communities. The nature of the future development of the former Richards-Gebaur Airport property is a primary issue of this plan.

Finally, there is an abundance of vacant, undeveloped, or agricultural land in the planning area. As one of Kansas City's few, remaining undeveloped areas south of the Missouri River, the future disposition of these properties is important.



Richards-Gebaur Airport



Intermodal facility



Undeveloped land

# PLAN PURPOSE

## and process

The continuing purpose for the Martin City Area Plan is to:

- Serve as the “Plan-of-Record” for the area;
- Provide a strategy for public infrastructure investments;
- Identify design guidelines to reinforce and enhance the Plan Area’s unique districts and sense of place;
- Provide a strategy for a transportation system that balances all modes;
- Guide future land use, development and zoning;
- Serve as a resource for developers, property owners, business owners, residents and interested citizens;
- Provide a strategy to revitalize existing neighborhoods and housing;
- Incorporate “sustainable” approaches to development to mitigate stormwater impacts and to enhance natural areas; and
- Outline an action plan for implementation.



## Planning Process

### THE MARTIN CITY/RICHARD-GEBAUR AREA PLAN (2001)

The planning process involved a two tiered public planning approach with the City’s Aviation Department undertaking a process to determine the future use of the former Richard’s Gebaur Airport and the City Planning and Development Department for the remainder for the study area. These efforts were on parallel tracks and resulted in a coordinated plan for the entire area.

During the initial planning process, a group called the PAC (Planning Advisory Committee) served as a “Steering Committee” for the plan. The PAC was composed of community leaders and stakeholders in the general planning area and at Richards-Gebaur specifically. Additionally, a series of community meetings was held with the general citizenry. The purpose of those meetings was to share information and give input on key planning decisions.

The early part of the process was dominated by aviation decisions – specifically the future of airport facilities at Richards-Gebaur. The PAC ultimately recommended that no future aviation facilities operate at Richards-Gebaur. With that decision made, planning and land use issues were addressed.



Several community meetings were held to allow the community at-large in order to provide input to the PAC and consultant team. These meetings provided basic “grass roots” participation for planning decisions.

The Aviation Department’s plan for the re-use of the former base was adopted by the City Council on April 19, 2001 by Resolution No. 010511. Two weeks afterword, the entire Martin City/Richards-Gebaur Area Plan was adopted by the City Council.

Since its adoption, there has been significant progress in implementing the policies of the plans. Examples include the continuing development of the former Richards-Gebaur Air Force Base to an intermodal hub and the formation of the Martin City Community Improvement District (CID).

### **MARTIN CITY AREA PLAN UPDATE (2008)**

During the fall of 2007 and spring of 2008, the City Planning and Development Department coordinated updates to the 2001 Area Plan concurrently with the Martin City Community Improvement District (MCCID) Master planning process. The affects of the update were limited and only intended to “fine-tune” the existing Plan to the City’s accepted Area Plan standards. Updates to the 2001 Area Plan included incorporating the City’s new future land use district categories and appropriate references to the City’s new Development Ordinance, adding appropriate references to the City’s Wet Weather Solutions Program, revising the Future Land Use Map based on recent or proposed developments that have occurred since 2001, and incorporating appropriate revisions based on the MCCID Master Plan.





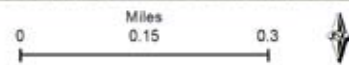
Businesses in the Martin City Community Improvement District.

# PLAN PURPOSE and process



**Legend**

-  Martin City Community Improvement District Boundary  
(Boundaries are current as of June 2008)
-  Parcels





## **MARTIN CITY CID MASTER PLAN (2008)**

In the fall of 2007 and early 2008 the Martin City Community Improvement District (MCCID) undertook a master plan for the development of the district and its surrounding environs (see map on previous page). The plan was closely coordinated with planned improvements to 135<sup>th</sup> Street between M-150 Highway and Holmes Road. The MCCID Master Plan process was intended to:

- Provide a vision for future development and redevelopment of the area;
- Guide development by recommending land uses, densities, priorities, and necessary improvements to support development;
- Establish an urban design plan with standards and guidelines for mixed-use design, pedestrian and transportation system improvements, streetscaping, and public spaces;
- Integrate storm water management strategies and guidelines stated in the City's on-going Wet Weather Solutions Program; and
- Promote the citywide initiatives established by the City's Comprehensive Plan, the FOCUS Kansas City Plan, as well as other city ordinances and initiatives.

The MCCID Master Plan was prepared with the assistance from the City Planning and Development Department and has been incorporated into the Martin City Area Plan.

## **Relationship to the FOCUS Kansas City Plan**

### **FOCUS COMPONENT PLANS**

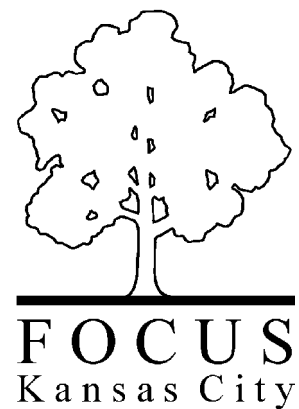
In Phase II, seven distinct but interwoven component plans were developed to detail the action steps needed to make the FOCUS vision a reality. On October 30, 1997, the FOCUS Kansas City Plan was adopted by Resolution 971268 as the comprehensive Plan for Kansas City, Missouri. The plan is made up of seven component plans, five of which contain aspirations, initiatives, and applications that are directly applicable to the planning area. The summary of the purpose of these plans are described briefly below:

#### **THE CITYWIDE PHYSICAL FRAMEWORK PLAN**

The Citywide Physical Framework Plan provides strategic land use planning guidelines, addresses the future character of growth, development and redevelopment, as well as capital needs of the city. Aspirations for enhancing natural features, encouraging multimodal transportation, promoting an efficient utilities system, and strengthening development patterns, provide a basis for all the plans that follow, including Area Plans.

#### **THE NEIGHBORHOOD PROTOTYPES PLAN**

The Neighborhood Prototypes Plan recommends specific actions to improve Kansas City neighborhoods and encourage resident partnerships in determining their future and delivery of city services. This plan identifies aspirations for neighborhoods that recognize connectedness, identity, and responsiveness. A unique neighborhood assessment process helps citizens target city services and specific strategies to their distinct requirements.



# PLAN PURPOSE

## and process



### THE PRESERVATION PLAN

The Preservation Plan highlights the importance of Kansas City's rich legacy of landmark structures, historic neighborhoods, and archeological resources that make the city a special place. This plan recognizes goals for preservation as identifying historic assets, utilizing assets economically, streamlining the regulatory process, increasing historic awareness, and utilizing history to promote tourism. Strategies on transportation, urban design, capital improvements, and tourism complete the vision of the future from a preservation perspective.



### THE URBAN CORE PLAN

The Urban Core Plan includes inventive strategies for central city neighborhoods, downtown, the Central Business Corridor, and plans for economic development, jobs, capital improvements, public transit, and neighborhood livability. Strategies for the location of cultural facilities, marketing the urban core, revitalizing and sustaining neighborhoods are outlined in this plan. This plan calls for linking activity centers, reaffirming neighborhoods, promoting life-long learning, creating great streets, enhancing urban amenities, recognizing culture as commerce, maintaining city cleanliness, and attracting good businesses, citizens, and jobs.



### THE NORTHLAND PLAN

The Northland Plan targets investment strategies to maintain existing neighborhoods and encourages development where public facilities (water, sewer, streets) already exist. Protecting the natural environment and current character of the Northland are fundamental to the plan. Specific transportation improvements are recommended to improve east-west traffic, extend the boulevard system north of the Missouri River and create pedestrian and bicycle-friendly thoroughways.



### THE HUMAN INVESTMENT PLAN

The Human Investment Plan outlines recommendations related to life-long education, retaining and encouraging diversity, equipping citizens for the changing work environment, job retention and expansion strategies, and programming for stimulating interest in culture and the arts. It also places emphasis on practical life skills for Kansas City's youth, and enhancing Kansas City as a place of excellence, creativity, celebration and unity. This plan aspires to create a city where people are healthy, productive, caring, and civil.



### THE GOVERNANCE PLAN

The Governance Plan sets out specific strategies for improving city services, establishing and maintaining the financial health of the city, strengthening metropolitan cooperation and political and organizational restructuring to insure implementation of the FOCUS initiatives. This plan contains initiatives and applications that encourage Kansas City to aspire in several different areas: city finance, city management, regional leadership, and citizenship.

## **FOCUS BUILDING BLOCKS**

The actions and initiatives of the seven component plans of FOCUS are linked through twelve interconnected strategies. These strategies, called the FOCUS Building Blocks, provide a guide for directing decisions, investments and community action in the Martin City area for the next 25 years. The following explains how the FOCUS Plan, through its twelve Building Blocks, relates to the planning area.

### **CITIZEN ACCESS AND COMMUNICATION**

Communication and access to information are critical elements for engaging citizens and making connections between citizens, businesses and government. This Building Block includes initiatives that will make information available and accessible to citizens, thus increasing communication among citizens, businesses, and government.

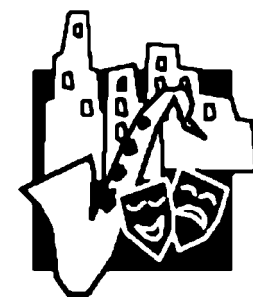
Creating a comprehensive directory of city services is an initiative from this Building Block that is applicable to the planning area. In addition, an expanded City Communications Office will disseminate important information to neighborhood and homeowners associations, block clubs, and other organizations wishing to receive information from the city. Such information includes meeting times and places, dockets for the City Plan Commission, the Board of Zoning Adjustment, the Landmarks Commission and other boards and commissions having decision-making authority over matters important to the planning area.

### **CITY LIFE**

Culture and entertainment enrich the quality of life in Kansas City. This Building Block recognizes the importance of culture and entertainment in the city. By creating excitement and celebrating diversity, "City Life" represents the strategy for making the Martin City area a unique place. This Building Block would be the basis for encouraging street festivals and identifying cultural hubs and places of entertainment that benefit the residents of the area.

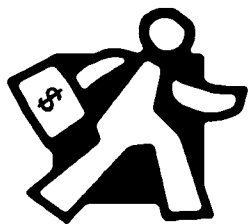
### **COMMUNITY ANCHORS**

Businesses, institutions, and organizations contribute to the identity, stability and growth of the neighborhoods that surround them. A "Community Anchors" program is a long range strategy for ensuring the well being and success of neighborhoods, businesses, and organizations that work together in a cooperative manner. "Community Anchors" in the area provide a variety of benefits including professional services, employment opportunities, community investment, and sponsorship of community activities.



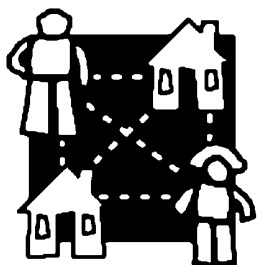
# PLAN PURPOSE

## and process



### COMPETITIVE ECONOMY

The Competitive Economy building block outlines strategies for providing Kansas Citians with the opportunity to thrive and succeed in a rapidly evolving and highly competitive economic system. Employment training, education, transportation, incentives, jobs development, and balanced business retention and assistance activities are some of the components of Kansas City's economic strategy. As an area of major employment, this Building Block is significant for the area.



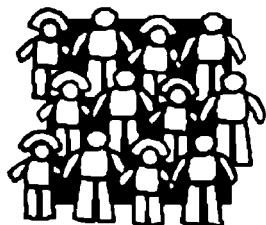
### CONNECTING CORRIDORS

Corridors link people, places, and activities. Beyond physical connections, corridors also link ideas, history, and culture. A single type of corridor cannot provide all of these connections. This Building Block includes a "corridor concept" that serves as a guide for the development and improvement of the various existing and future corridors in Kansas City. Such 'connecting corridors' in the planning area may include 135th Street, Holmes, M-150, and the Blue River Corridor, and others.



### FOCUS CENTERS

FOCUS Centers are a way of providing citizens with information and services at the neighborhood level. Through partnerships among the city, businesses, anchors, and community groups, FOCUS Centers will provide the services and information needed by citizens in Kansas City and the area.



### HEALTHY COMMUNITY

The Healthy Community Building Block recognizes that people cannot achieve their full potential when they encounter health problems, homelessness, racial intolerance, and other barriers. "Healthy Community" incorporates "partnership" and "prevention" as strategies to encourage people in Kansas City to be productive and caring.



### INVESTING IN CRITICAL RESOURCES

Commitment of resources and investments in basic and essential services is one of the highest priorities for Kansas City. This Building Block outlines the City's fundamental responsibilities in this area. These are: to protect the lives and property of Kansas Citians; to be responsible stewards of the public's capital assets; and to safeguard the natural environment. With both natural and built environmental issues, this Building Block is important for the area.



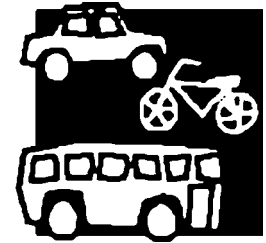
### LIFE LONG LEARNING

Life long Learning establishes strategies providing people with learning opportunities throughout their lives. By focusing on people at all stages of their lives, life long learning ensures that Kansas City will become a model for "Citizens Involved in Learning".



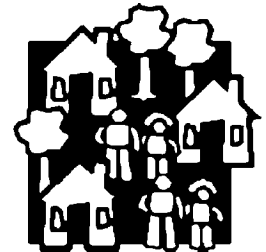
## MOVING ABOUT THE CITY

The ability to move people and goods throughout the city, as well as to connect all locations within the city, is essential to the economic, social, and cultural success of Kansas City. The “Moving About the City” Building Block represents a transportation strategy and emphasizes reliability, efficiency, and choice. Incorporating pedestrian-friendly amenities into new development and the creation of better transit are ways to reinforce this in the area.



## NEIGHBORHOOD LIVABILITY

Livable neighborhoods connect people both physically and socially. A neighborhood’s physical character, its residents and its history, give it identity and make it unique. In a livable neighborhood, houses are in good condition, and residents are healthy, productive and responsible citizens. Physical infrastructure is in good condition, and services are readily available and accessible. These and other characteristics of a livable neighborhood are addressed in this Building Block. The Martin City area has both existing and emerging neighborhoods that must be considered.



## QUALITY PLACES TO LIVE AND WORK

The physical development of the city is related to people, their needs, and the character of Kansas City. This Building Block provides guidelines to ensure development that establishes a good relationship between Kansas Citians and the various physical elements of the city, including residential and commercial areas, focal points, activity centers, landmarks, corridors, waterways, and other elements. The natural beauty, existing assets, and development potential of the Martin City area make it a suitable area as a “Quality Place to live and work”.



FOCUS emphasizes connections - connecting people to places, people to each other, and the past to the future. If Kansas Citians can act as one connected city with an optimistic vision, a unifying strategy, and clear action steps, the end result will be a city that works well for people. This is also what the Martin City Area Plan aspires to achieve: a connected community, where business and residents live and thrive together, working towards common goals with a positive attitude and strategies.

**MARTIN  
CITY**

# EXISTING CONDITIONS summary

## Introduction

The following is a summary and evaluation of Existing Conditions for the Martin City area. This summary provides a backdrop of information and analysis critical to proposed planning recommendations.

## Study Area

The configuration and location of the study area presents a number of planning and implementation challenges. Although it is located within the incorporated boundaries of Kansas City, Missouri, the area is surrounded on three sides by neighboring municipalities (Grandview, MO; Belton, MO; Leawood, KS; and Overland Park, KS). Therefore coordination with surrounding jurisdictions is critical when making planning recommendations in the study area. Land use decisions and impacts often cross jurisdictional boundaries.

## History

The south Kansas City region has a long and rich history. Early development in what is now the 6th City Council District occurred along the relatively flat lands of the Blue Ridge, between the Big Blue and Little Blue Rivers, along the higher lands on both sides of the valley. The Santa Fe Trail crossed the district from Raytown to State Line Road.

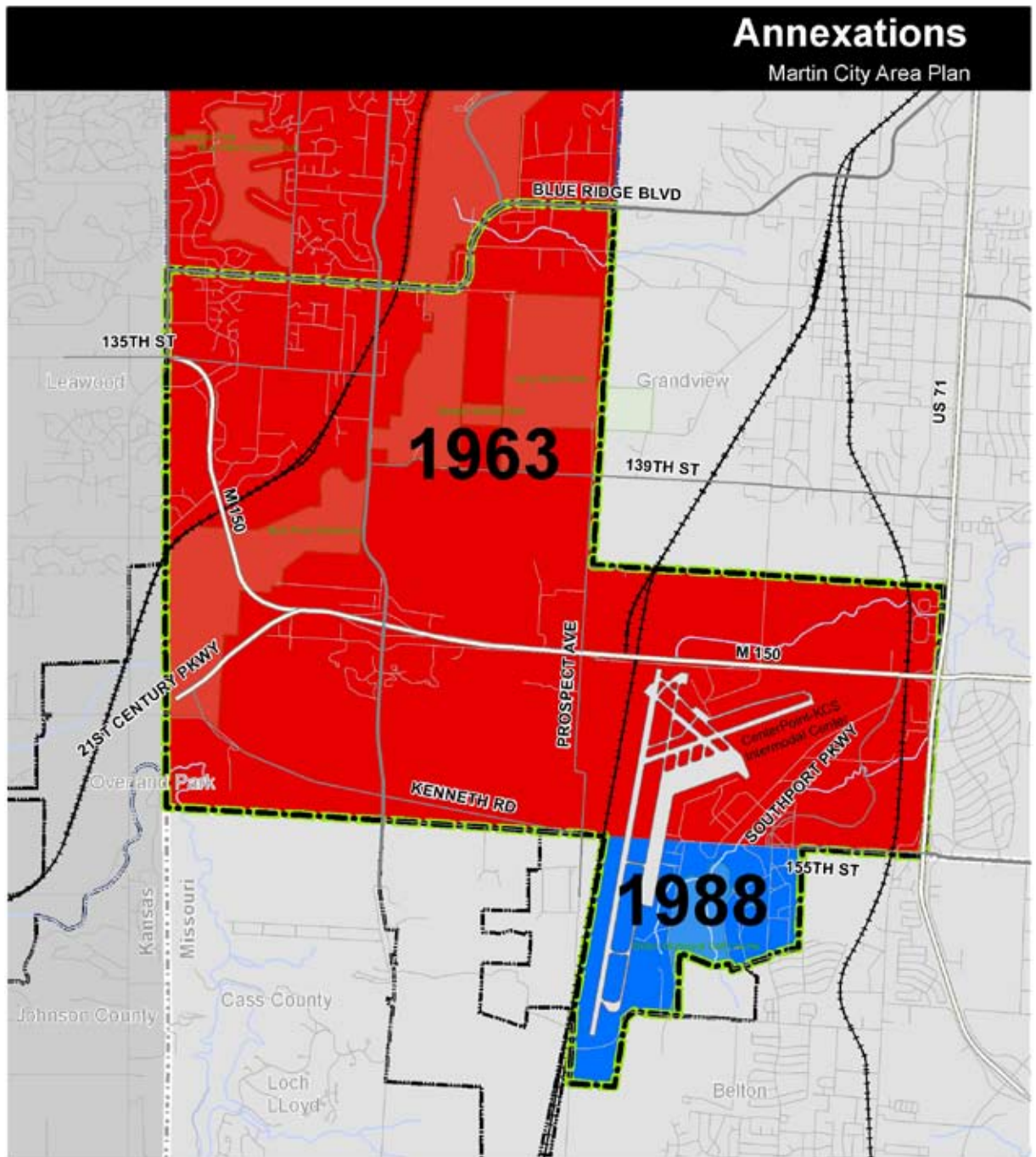
The City of Kansas City, Missouri annexed much of the area that is now the 6th District in 1947 and later. Further annexation took place between 1958 and 1963. In 1961, Kansas City annexed the area east of the 1958 annexation to the Lee's Summit limits and encircled the city of Raytown. The 1963 annexation included all of the area south of Longview Road to the Cass County line on either side of Grandview.

Development in the 6th District occurred as a general extension of development in Kansas City to the southwest along the ridgelines. In addition, with the completion of the interstate highway system within the 6th District, suburban developments, like Bridlespur and Somerset Hills north of the planning area were built, providing short commute times to work outside of the district. Strong institutional and commercial growth, such as St. Joseph Hospital and the Bannister Mall/Benjamin Plaza retail complex, continued into the 1990s.

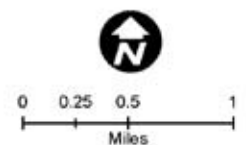


Early settlers of the area

# EXISTING CONDITIONS summary



- Study Area
- Local Streets
- Railroads
- Community Improvement District
- City Limit
- County Boundary



## MARTIN CITY

The town of Martin City, MO, located in the vicinity of I35th and Holmes Streets was originally named Tilden. It was platted January 4, 1887 by E.L. Martin and John H. Lipscomb. The town of Tilden was named for Samuel Jones Tilden, a presidential candidate who was defeated by Rutherford B. Hayes in 1876 (he lost by one electoral vote). The town name was changed in 1895, most likely because there was another town of Tilden in Dallas County, MO.

The name Martin City came from Edward L. Martin who organized the Kansas City Distilling Company and E.L. Martin & Co. wholesale liquor. Mr. Martin was Mayor of Kansas City in 1873 and served on the Board of Education from 1875 to 1896. He was also a partner with Arthur Stilwell in the Kansas City, Pittsburgh & Gulf railroad, known today as Kansas City Southern.

In the early 1900s, Martin City had a train depot, a hotel, a blacksmith shop and a general store to serve local residents. In 1911 a bank opened to accommodate townspeople and similar to many banking services that declined during of the Depression, the Martin City bank closed in 1933. Since 1992 the building has been used as a hand crafted ceramics shop.

Martin City was full of activity in the roaring 1920s. During Prohibition, one resident recalled, there was drugstore in Martin City where customers could “get a little nip” at a window in back of the store. The dry goods store had a dance hall upstairs where children and adults could roller-skate.

In 1957 a tornado destroyed much of the area. However, the community rebuilt and in subsequent years many of its small town businesses became regional destinations for the metropolitan area. In 1963 the area was annexed by the City of Kansas City (see annexation map).



Damage to Martin City from 1957 tornado



# EXISTING CONDITIONS summary

## THE RICHARDS-GEBAUR AIRPORT

The former Richards-Gebaur Airport was originally opened as the Grandview Airport in 1943. In 1952, the United States Air Force acquired it and later named it for two local officers killed in combat. The Continental Air Command was transferred from New York to Richards-Gebaur Air Force Base in 1953.

Beginning in the late 1960s, the United States Air Force began phasing out the base and eventually reduced Richards-Gebaur to a small tactical wing. In 1976, the Federal Government deactivated the Air Force Base as an active military facility, although more than 240 acres were retained by the United States Air Force as a reserve station. Title to the remaining portion of the base, some 1,100 acres, was transferred to the City of Kansas City, Missouri, in 1985. The City annexed the property, which included part of Cass County, in 1988.

In 1991, the Federal Government deactivated the Air Force Reserve Station with all military operations ceasing in 1994, and much of the remaining property was acquired by the City of Kansas City, Missouri in 1995.

In recent years, the City and Port Authority have been working to attract private developers for the site. In 2003 plans were announced that Centerpoint would develop the site as an inland port. The City began selling portions of the site in 2007 to Centerpoint for that purpose. The development will bring a 370-acre intermodal facility a 970-acre industrial park to the Martin City area.

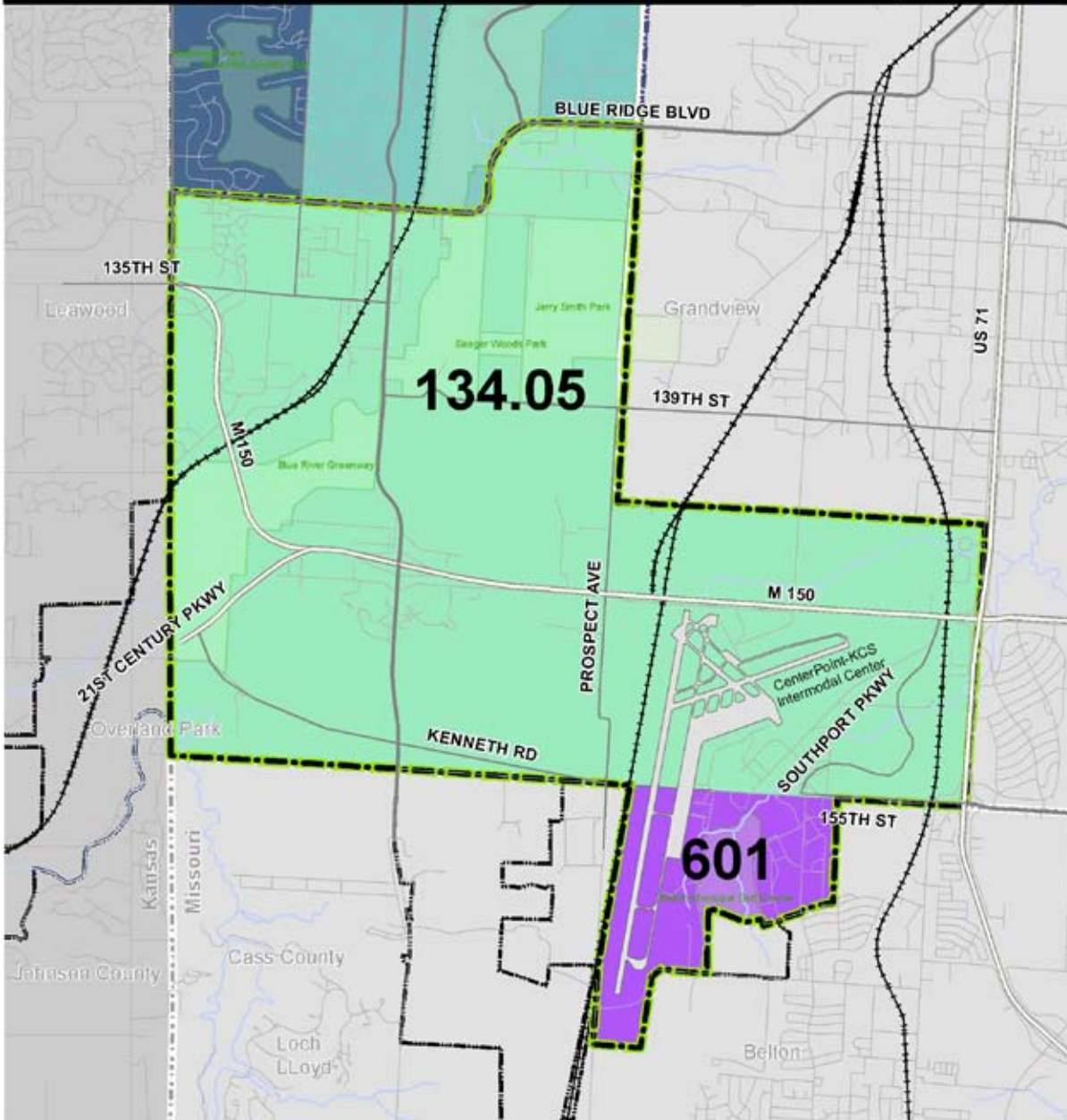
## **Census Profile**




The following information is an overview of demographic characteristics of the area. The data discussed here is provided by the US Census Bureau (1980, 1990 and 2000 Decennial Censuses) and the City Planning and Development Department (2007 estimates). The accompanying map illustrates the census tracts used in this analysis.

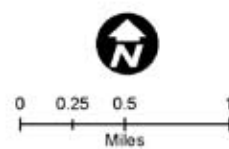


## Census Tracts

Martin City Area Plan



-  Study Area
-  Local Streets
-  Railroads
-  Community Improvement District
-  City Limit
-  County Boundary



# EXISTING CONDITIONS summary

## CHANGE IN POPULATION

- Population growth in the area since 2000 has been significant. The estimated population in 2007 for the area is 2,288, up 22% from 2000. This growth can be attributed to the large amount of new residential development in recent years, mostly in the northern portion of the planning area (north of 135th Street).

## RACE

- The majority of the population in 2000 was white (60%), while just over 1/3 (36%) was black. The remaining population represents a variety of ethnic groups.

## HOUSING CHARACTERISTICS

- According to 2007 estimates, the area added 232 new housing units since 2000, an increase of almost 30%.
- Vacancy is low (more than 90% of the units are occupied). Of the occupied units, most (58%) are renter occupied
- Single family homes are the predominant housing type in the area, accounting for nearly 2/3 of area housing units.
- Homes in the area are relatively new (nearly 1/3 constructed since 2000) and home values compare favorably to the metropolitan area (median value slightly above metro area).



## EMPLOYMENT

Estimates for 2007 indicate a growth of 880 jobs in the area since 2000, with an additional 120 jobs in the area forecast by 2010. The Martin City area is projected for large growth in employment, with more than 2,800 jobs created by 2020, or an increase of 40%. Similar levels of job growth are predicted for the surrounding areas (Grandview, Belton and Leawood).

Table 1 - Employment

Employment Sector		ACTUAL		ESTIMATE	FORECAST		
		1990	2000	2007	2010	2020	2030
Manufacturing	No. of Jobs	1,000	1,600	1,880	2,000	2,800	3,500
	% Change	-	60%	17.5%	25.0%	40.0%	25.0%
Services	No. of Jobs	1,300	1,200	1,130	1,100	1,100	1,100
	% Change	-	-7.7%	-5.8%	-8.3%	0.0%	0.0%
Retail	No. of Jobs	500	500	500	500	1,200	1,200
	% Change	-	0.0%	0.0%	0.0%	140.0%	0.0%
Other	No. of Jobs	600	200	130	100	-	-
	% Change	-	-66.7%	-35.0%	-50%	-100%	-
Total	No. of Jobs	3,300	3,400	3,610	3,700	5,200	5,800
	% Change	-	3.0%	6.2%	8.8%	40.5%	11.5%

The census tract level data file upon which the above tabulations are based represents a disaggregation of Mid-America Regional Council forecasts done originally for whole census tracts, not dividing along the city limits of Kansas City, Missouri. Data for tracts split by those city limit lines were disaggregated by the City Planning and Development Department to allow separate analysis of areas inside and outside of Kansas City, Mo. The disaggregation process was based upon proportions of 1990 employment by each type inside and outside the city in each split census tract.

Forecasts rounded to the nearest hundred. All forecasts assume there will be no major economic, social, or environmental disruptions and that energy and natural resources continue to be available at reasonable prices.

## AREA PROFILE

Source: 1980, 1990 and 2000 Censuses; and Kansas City Planning and Development Department 2007 Estimates

POPULATION CHARACTERISTICS	NUMBER				PERCENT			
	1980	1990	2000	2007	1980	1990	2000	2007
<b>Persons</b> (percents are % change from 10 years previous or 2000-7)	1,411	1,833	1,869	2,288	10.1	29.9	2.0	22.4
<b>Sex</b>								
Male	667	902	897	1,101	47.3	49.2	48.0	48.1
Female	744	931	972	1,186	52.7	50.8	52.0	51.9
<b>Race and Ethnicity</b>								
White	1,254	1,583	1,135	1,394	88.9	86.4	60.7	61.0
Black	91	201	666	756	6.4	11.0	35.6	33.0
Other Races	66	49	68	138	4.7	2.7	3.6	6.0
Hispanic Origin (of any race)	25	35	77	266	1.8	1.9	4.1	11.6
<b>Median Age</b> (percents are % of metro median)	28.9	25.7	23.5	23.7	95.3	78.3	66.8	63.4
<b>Unemployment</b> (among civilians 16 or older in labor force)	65	54	78	116	10.4	6.1	9.4	11.7
<b>Persons 25+ with at Least a High School Education</b>	479	704	622	838	59.9	78.0	73.8	78.6
<b>HOUSING CHARACTERISTICS</b>								
<b>Units</b> (percents are % change from 10 years previous or 2000-7)	598	728	777	1,009	47.7	21.7	6.7	29.8
<b>Type</b>								
Single Family	316	405	424	648	52.8	55.6	54.6	64.3
Multifamily	282	323	353	360	47.2	44.4	45.4	35.7
<b>Year Created</b> (as estimated by inhabitants or conversion year)								
Before 1940	60	33	58	58	10.0	4.5	7.4	5.7
1940 to 1969	345	326	366	357	57.7	44.8	47.1	35.4
1970 onward	193	369	354	594	32.3	50.7	45.5	58.9
<b>Value</b> (in constant 2007 dollars)								
Under \$50,000	17	4	17	3	9.5	1.7	7.3	0.7
\$50,000 to \$100,000	60	43	30	103	33.9	19.6	12.6	27.3
\$100,000 or More	100	174	187	272	56.5	78.7	80.1	72.0
Median Value (percents are % of metro median)	\$110,399	\$135,728	\$130,546	\$151,056	126.1	154.2	124.3	102.4
<b>Average Rent</b> (in 2007 dollars; percents are % of metro average)	\$ 361	\$ 648	\$ 447	\$ 452	91.4	137.6	77.9	64.8
<b>Occupancy</b>								
Owner-Occupied (percents are % of occupied units)	208	250	254	378	38.2	36.8	34.2	41.3
Renter-Occupied (percents are % of occupied units)	337	429	489	537	61.8	63.2	65.8	58.7
Vacant	53	49	34	94	8.9	6.7	4.4	9.3
<b>HOUSEHOLD CHARACTERISTICS</b>								
<b>Households</b> (%'s are % change from 10 years previous or 2000-7)	545	679	743	915	40.8	24.6	9.4	22.4
Family Households	396	499	504	593	72.7	73.5	67.8	64.8
Non-Family Households	149	180	239	322	27.3	26.5	32.2	35.2
<b>Average Persons per Household</b> (percents are of metro average)	2.48	2.60	2.43	2.45	93.9	102.1	96.6	97.5
<b>Median Income</b> (in 2007 dollars) (percents are % of metro median)	\$ 36,182	\$ 36,143	\$ 33,714	\$ 30,728	87.9	86.0	72.8	57.7
<b>Moved in During Previous 5 Yrs.</b>	321	432	486	535	58.9	63.6	65.4	58.4

Kansas City, MO Planning and Development Department

# EXISTING CONDITIONS summary

## Natural Environment

The natural environment of the planning area is characterized by rolling terrain, drainage ways, and an abundance of vegetation. The Blue River Corridor bisects the area, providing a primary drainage way and a significant flood plain.

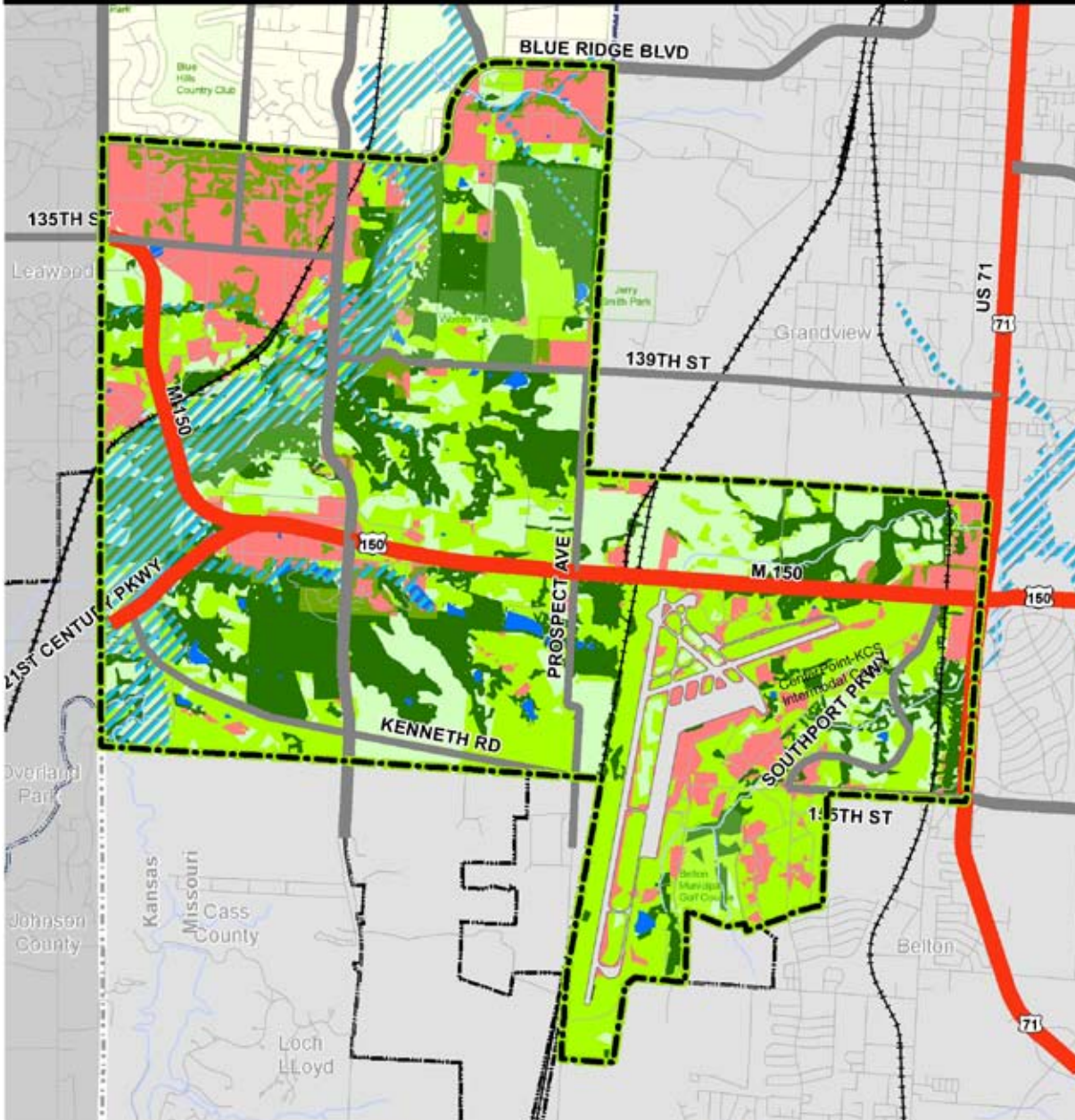
As a developing district, the Martin City planning area is more notable for its natural features than for its built environment. Natural features abound in the study area. They consist primarily of waterways, wooded lands, and scenic views. A challenge for the future of the study area is to accommodate development while recognizing and preserving the value of these natural features





## Natural Resources

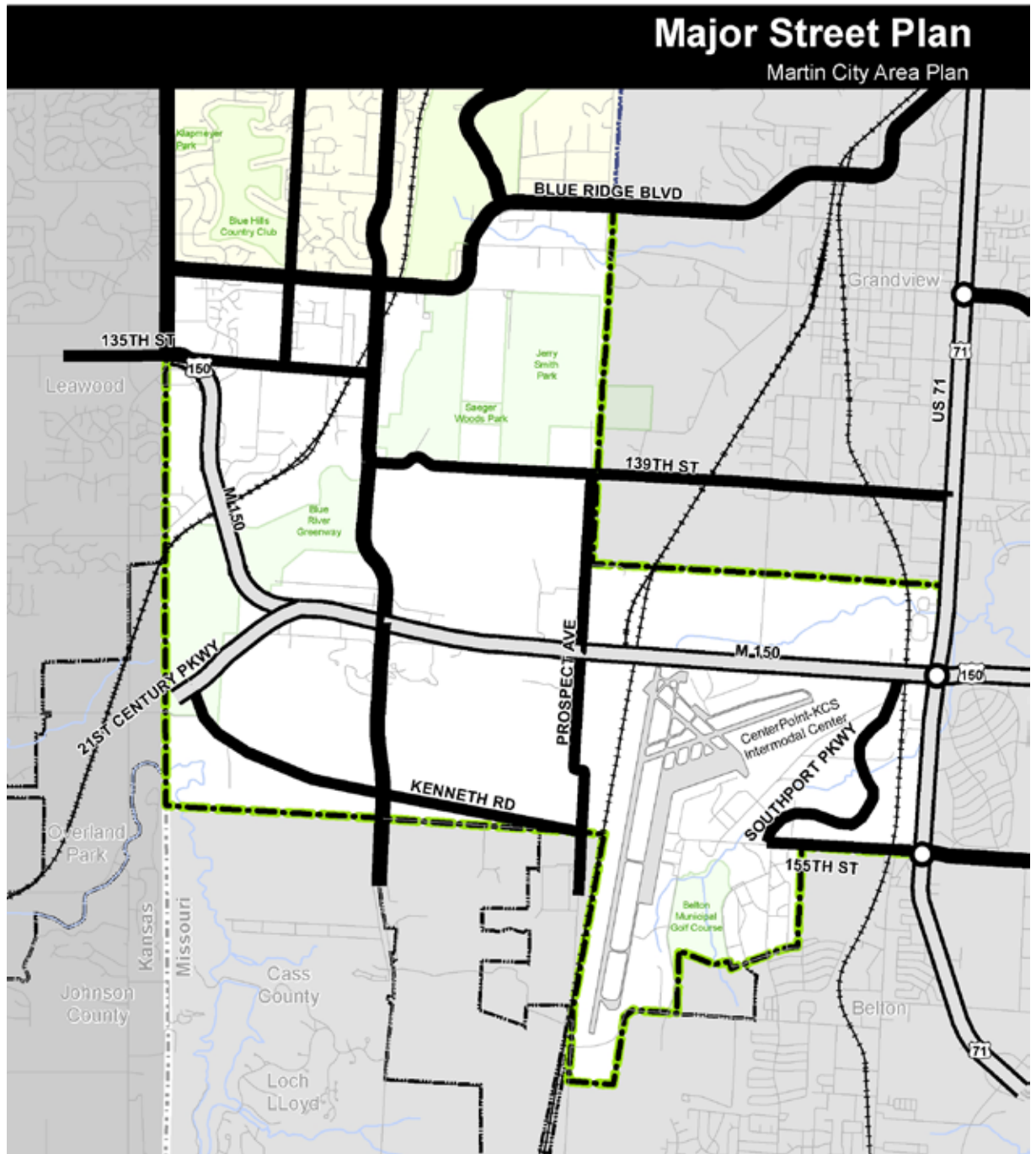
Martin City Area Plan



- Local Streets
- Railroads
- Community Improvement District
- City Limit
- County Boundary
- Agriculture; Cultivated Land
- Grassland
- Forest & Woodland (Urban)
- Forest & Woodland
- Developed Land
- Wetland & Marsh
- Open Water
- Secondary Floodplain
- Primary Floodplain



# EXISTING CONDITIONS summary



**Major Street Plan**

Study Area	Local Streets	Railroads	Partial Interchange	Secondary Arterial
Community Improvement District	City Limit	County Boundary	Full Interchange	Primary Arterial
				Expressway

0 0.25 0.5 1 Miles





The new Highway 150

## Transportation System

### ROADWAY TRANSPORTATION

As an emerging district, the planning area does not yet have a fully developed arterial network. The Major Street Plan, approved by the City Council in 1971, identifies general alignment and functional capacities of the arterial network for Kansas City, Missouri. Currently, the following arterials serve the community: Holmes Road, 135th Street, 155th Street, Blue Ridge Boulevard, Botts Road, 139th Street, Prospect Ave, and Kenneth Road. U.S. Highway 71 and the recently completed M-150, categorized as “Freeways”, provide good access to the area and to the regional highway network.

In addition, although designated by the major street plan, there are a number of roads that are not yet built and other existing roadways that are not yet developed to the City’s arterial standards that include:

- Improvements to Botts road, North of M-150
- Creation of 21st Century Parkway connecting new development southeast of the former Richards-Gebaur Airport.
- A future boulevard extension within the area by the Parks, Recreation and Boulevards Department.
- Undertake an engineering study to re-evaluate future improvements to Holmes Road, including a proposed railroad overpass.
- Development of a roadway system designed to improve access within and through the Centerpoint Intermodal facility.

See Chapter 3 for recommended changes to the Major Street Plan.

# EXISTING CONDITIONS summary

## RAIL LINES

The two active rail lines significantly influence the development character within the planning area. There are several rail/auto conflicts in the area, the most significant of which is at the Union Pacific Line and Holmes Road/135TH Street near Martin City.

## TRANSIT SERVICE

Currently, the Kansas City Area Transportation Authority (KCATA) serves the area with limited, commuter express bus service on weekdays.



## PEDESTRIANS

A well designed pedestrian system is critical for both neighborhoods and mixed-use centers, as well as providing access to future transit networks in the area. Currently, the pedestrian network in the area, like the arterial street network, is minimal.

## TRAILS AND BIKEWAYS

There are currently no bike routes or trails in the area. However, several are planned. The Bicycle Routes & Citywide Trails Plan map depicts proposed Bike Routes (as designated in the Major Street Plan) and proposed regional trails in the area (from the Citywide Trails Plan). As part of the area plan update, additional “neighborhood connector” trails are proposed for the area. These trails should be constructed as new roads are built and as development occurs.



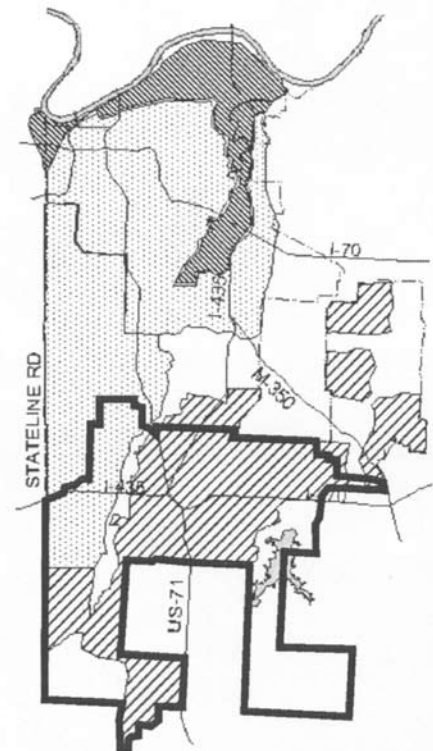
## Utilities

Major sewer service and water service trunk lines are currently sporadic in the planning area. The northern portion of the area is served by trunk sanitary sewer lines, as well as land just to the east of the former Richards-Gebaur airport. Major trunk distribution water mains also serve the northern portions of the planning area and the Richards-Gebaur property.

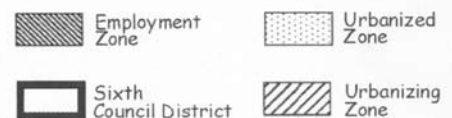
The provision of utilities was a major component in the designation of Priority Development Zones in the FOCUS Plan. The accompanying map indicates that the southwestern portion of the planning area is located in a Development Priority Zone.



## LAND USE AND DEVELOPMENT



*Development Priority Zones*



# EXISTING CONDITIONS summary



Large estate lots



135th Street commercial



Equestrian/polo fields

## Land Use

One of the more significant physical characteristics of the Martin City area is the amount of vacant and undeveloped land within the city limits. Development has typically occurred in areas of opportunity, where flat and easily cleared and graded parcels were available.

Developed areas are typically located in the northwest portion of the study area, near Blue Ridge Boulevard, State Line Road, I35th Street, and Holmes Road. East and south of this area, development typically is low density residential, estate lots, or agricultural and open space.

Commercial uses are concentrated along State Line Road, M-150, I35th Street and near Holmes Road, and are typically single use, single site, and automobile-oriented. Industrial uses generally are concentrated in two areas - Martin City (south of I35th Street) and on the former Richards Gebaur Airport (the Centerpoint-KCS Intermodal facility). They often lack transitions or buffering from adjacent uses.

Generally, agricultural/vacant lands within the project area appear to be utilized for crops, pasture, and other uses such as equestrian facilities, kennels, and similar uses (predominantly located south and east of the Blue River). Significant wooded areas have been preserved along the Blue River Valley. Within these areas, park facilities have been developed, which range from simple shelters and ball fields, to equestrian/polo fields.

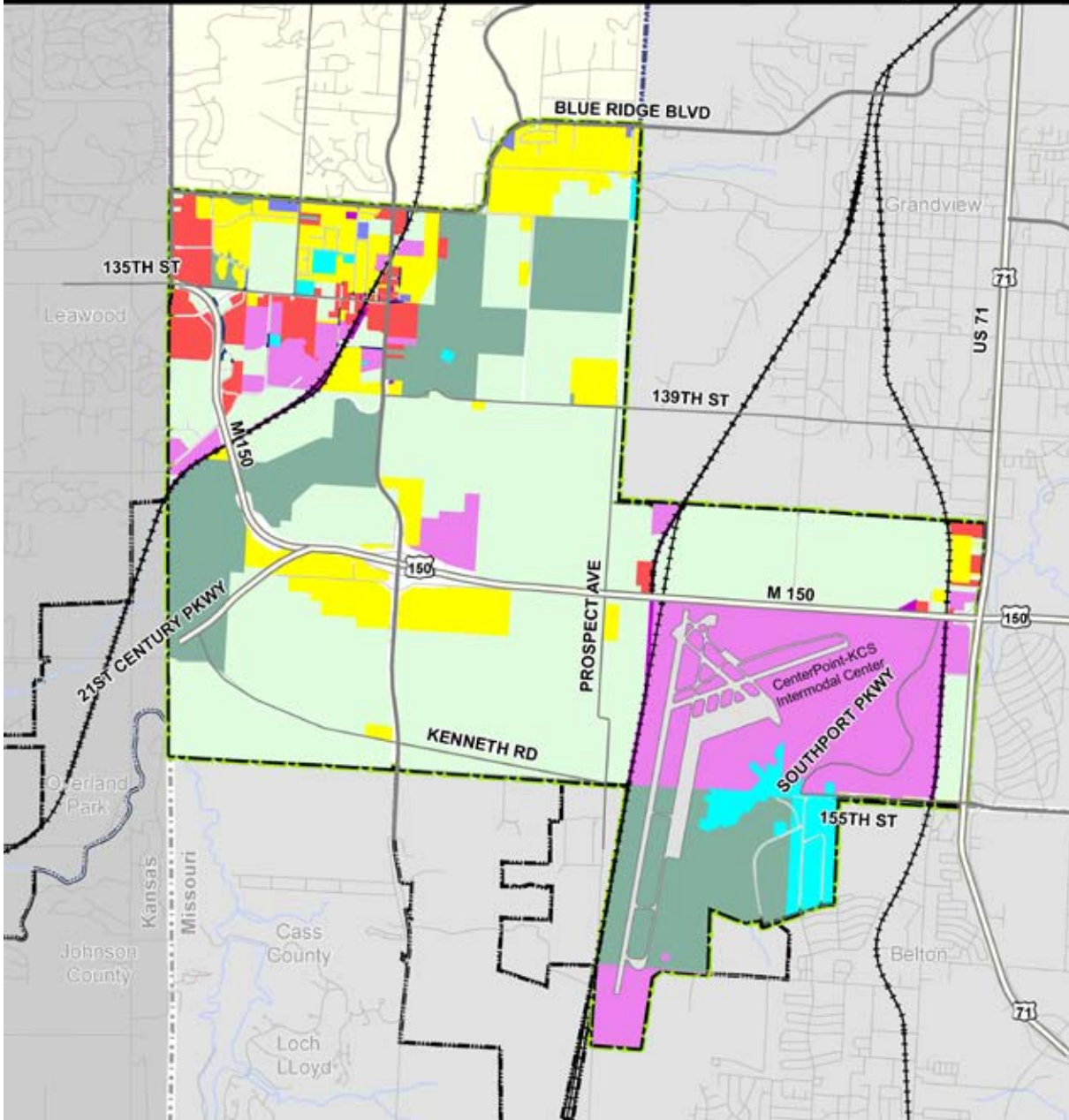
Table xxxx

Land Use	Acres	Percent	Parcels	Percent
Agricultural	2,815	40.5%	95	8.9%
Industrial (Light)	1,274	18.3%	39	3.7%
Parks and Recreation	1,236	17.8%	15	1.4%
Open Space	589	8.5%	209	19.6%
Residential Single Family	487	7.0%	524	49.2%
Commercial	233	3.4%	70	6.6%
School, Library, Church, Public	110	1.6%	14	1.3%
Institutional	66	1.0%	9	0.8%
Residential Multi Family	59	0.8%	58	5.5%
Transportation	58	0.8%	11	1.0%
Office	8	0.1%	9	0.8%
Industrial (Heavy)	5	0.1%	1	0.1%
Utility	5	0.1%	2	0.2%
Parking	3	0.0%	8	0.8%
<b>Total</b>	<b>6,948</b>	<b>100%</b>	<b>1064</b>	<b>100%</b>

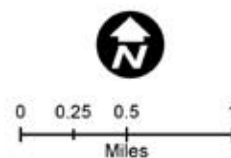


## Current Land Use

Martin City Area Plan



### Current Land Use



# EXISTING CONDITIONS summary



Agricultural zoning



Low density residential zoning

## Zoning

There are four general zoning districts within the area: Agricultural, Industrial, Residential, and Commercial.

The area is predominantly zoned agricultural (R-A, Agricultural). Generally, allowable R-A district uses include agricultural, general farming, kennels and residential.

Industrial zoning is the most significant district within the area, concentrating at and around the intermodal facility and in the area south of I35th Street between Holmes Road and the State Line. Much of the existing industrial zoning in the area south of I35th Street is not consistent with the current use of the properties and the long range future land use proposed for the area. District classifications range from MI (light industry) to MP (planned industry).

Commercial zoning is generally concentrated along State Line Road and M-150, and Holmes Road, the intersection of Holmes Road and I35th Street, and I35th Street and State Line Road. District classifications range from CI (neighborhood retail) to CP-3 (planned business centers), allowing a wide-range of commercial development types.

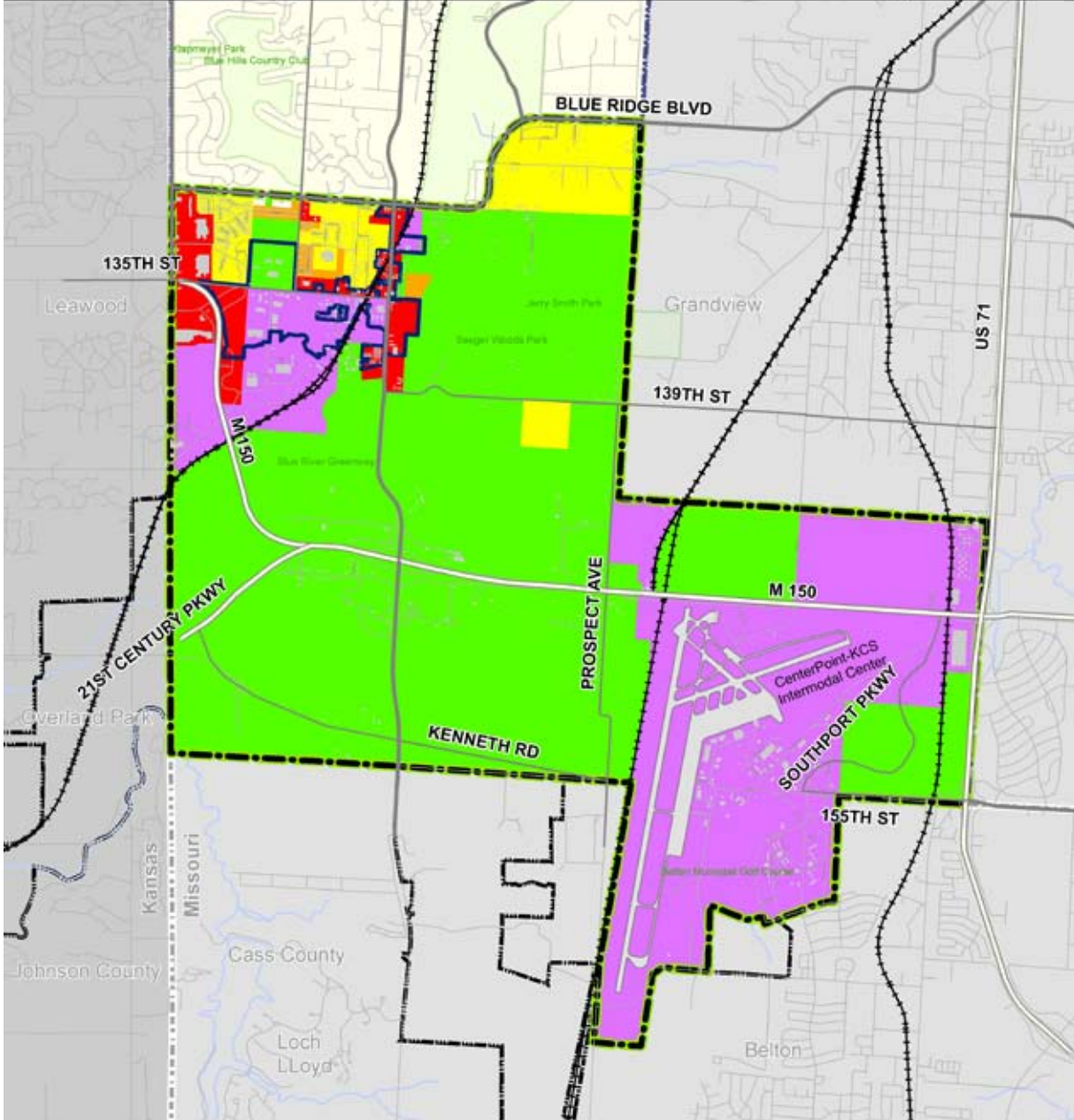
Residential zoning is generally located north of I35th Street and along the Blue Ridge Boulevard. District classifications range from R1a (medium density residential) to R5P (planned high-rise apartments).

Table xxxx

Zoning Category	Acres	Percent of Area
Agricultural/Vacant	14,011	78.8%
Industrial	2,361	13.3%
Residential Single Family	1,064	6.0%
Commercial	295	1.7%
Residential Multi Family	44	0.2%
<b>Total</b>	<b>17,775</b>	<b>100.0%</b>



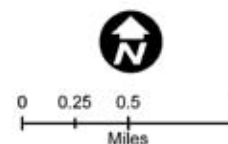
## Zoning Martin City Area Plan



- Study Area
- Local Streets
- Railroads
- Community Improvement District
- City Limit
- County Boundary

### Zoning Classification

- Multi Family Residential
- Industrial
- Single Family Residential
- Commercial
- Agricultural/Vacant



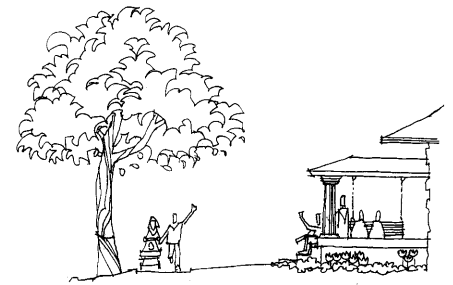
MARTIN  
CITY

# RECOMMENDATIONS

## Planning Goals

The following planning goals were identified in the *Martin City-Richards Gebaur Area Plan* (2001) and have been reaffirmed during the Martin City Area Plan Update (2008) by both the Steering Committee and during public workshops. General goals are organized into four categories and more specific objectives are identified for each of the four goals.

- I. Future projects should adhere to smart growth principles to promote growth and development that creates a quality-oriented environment-conserving natural resources and providing community amenities.
  - Plan for and phase growth in an orderly manner, based on “smart growth” principles and their relation to the plan’s established development priority zones.
  - Establish a high-quality, village-like, lower density, mixed-use neighborhood environment in areas delineated in the plan’s future land use map.
  - Develop residential neighborhood amenities such as parks, public areas, trails, and community services.
  - Provide convenient locations for goods and services oriented to the local community.
  - Provide buffers, transitions, and open spaces between non-compatible uses through the use of environmentally sensitive development practices as described in this document.
  - Maintain a healthy environment with high air and water quality, as well as a habitat for wildlife by utilizing stream buffers as defined in the draft Kansas City Development Ordinance.



*Smart Growth is an urban planning and transportation theory that concentrates growth in the center of a city to avoid urban sprawl; and advocates compact, transit-oriented, walkable, bicycle-friendly land use, including neighborhood schools, streets that work for everyone, mixed-use development with a range of housing choices.*

*Smart Growth values long-range, regional considerations of sustainability over a short-term focus. Its goals are to achieve a unique sense of community and place; expand the range of transportation, employment, and housing choices; equitably distribute the costs and benefits of development; preserve and enhance natural and cultural resources; and promote public health*





# RECOMMENDATIONS

2. Provide infrastructure and a transportation network that both adequately serve the existing community and future development within the area's "Development Priority Zones." This area is generally characterized as being contiguous to existing development and utilizing existing infrastructure.
  - All public infrastructure and public utility requirements must be satisfied for all future development projects.
  - Expand the area's parks, recreation, and open space system.
  - Improve the vehicular transportation system throughout the community, including the area's local roadway system, rail and automobile conflicts, and better access for emergency services.
  - Implement a system of collector streets as development occurs (constructed/improved by the developer) to relieve pressure on arterial streets and ensure adequate circulation and connectivity. Spacing of new collector streets should generally be 1/4 to 1/3 mile
  - Avoid the negative impact of "over-the-road" vehicles through residential neighborhoods.
  - Work with the state government and adjacent communities to improve state highways including Missouri Highways 58 and 150 and US Highway 71.
  - Implement a system of on and off street bike routes and multi-use trails (see Trails and Bike Routes Map).
3. Protection of the Environment to secure environmentally sensitive development throughout the planning area, particularly large scale development such as industry.
  - A clean, healthy environment is important to the future of the Martin City area, as well as all of Kansas City and the surrounding metropolitan region. Maintaining a healthy environment, with respect to air, water and land quality, is a basic goal of this plan. The development patterns that are proposed by this plan will encourage:
    - ✓ The use of alternative modes of transportation thus reducing the average vehicle miles and vehicle emissions;
    - ✓ Clean development and industry attraction to the area.
    - ✓ Preservation of quality open space and stream buffers where appropriate as development occurs.
  - Integrate Stormwater Best Management Practices (BMP's) - New projects within the Plan Area will adhere to the following KC- ONE Wet Weather Solutions Program Guiding Principles:
    - ✓ Through strong creative leadership and a stewardship ethics, the Wet Weather Solutions Program will take action to manage the City's water resources in a sustainable way.
    - ✓ Watershed-Based: Considers all sources of problems and solutions so that strategies account for the interrelationship of water, land use, air quality, and human communities within a watershed, leading to project outcomes with multiple benefits.
    - ✓ Maximize environmental, community and economic benefits: Prefer options that create multiple benefits for the community, environment, and the regional economy to create a stronger, more appealing, and a more prosperous community.
    - ✓ Financial: Manage the community's resources with a long-term view, pursuing fairness in the distribution of the economic benefits and burdens.
4. Develop a high level of communication within the community regarding planning and development issues.
  - Establish and maintain an ongoing communication mechanism between government agencies and neighborhood and community organizations.



## Future Land Use

The following land use descriptions serve as a guide for future growth and development within the plan area by outlining recommended uses and densities for each area. The land use designations are for planning purposes and do not represent a change to existing zoning.

**Residential Low:** Primarily intended for single family detached residential development, but allows a variety of residential building types up to 5.8 units per acre. This land use classification will correspond to the R7.5 and R-10 zoning categories within the new zoning ordinance.

**Residential Medium:** Intended for a variety of building types that allows up to 8.7 units per acre. This land use classification will correspond to the R.5, R.6 and R 7.5 zoning categories within the new zoning ordinance.

**Residential Medium-High:** Includes small lot single-family development, town houses, duplexes and apartments up to 17.4 units per acre. This land use classification will correspond to the R2.5 zoning category within the new zoning ordinance.

**Commercial:** Primarily intended to accommodate “heavier” commercial activities and/or operations that are not found in or compatible with mixed-use neighborhood oriented environments. This land use classification will correspond with the “B4” zoning category within the new zoning ordinance. Includes large-scale commercial development targeted in designated areas along major arterials with highway access.



Commercial

**Office:** Primarily intended to accommodate professional, administrative and corporate office uses (uses that require a large public interface should be reserved for Commercial and Mixed-Use areas). This land use classification will correspond with the 01 and 02 zoning categories within the new zoning ordinance.

**Parks:** Public or private land reserved for parks and parkways that is intended to accommodate active and passive parklands, trails, recreational uses, environmentally sensitive areas, or any other lands reserved for permanent open space purposes.



Low Density Residential



Medium Density Residential



Medium-High Residential



Office



Parks



Open Space/Buffer



Conservation District



Institutional



Light Industrial



Mixed-Use Neighborhood



Mixed-Use Community

# RECOMMENDATIONS

**Open Space/Buffer:** Consists of private or public lands that are in some way either temporarily or permanently reserved from development, including lands unsuitable for development. This includes but is not limited to creeks and stream buffers, floodplains, woodlands, severe slopes, and buffer zones around natural resources (areas difficult for development due to topography, hydrology, aged woodlands, archeological findings, etc.)

**Conservation District:** Conservation Districts are intended to encourage flexibility in design standards (example: reduced lot sizes or increased density) in exchange for 60% open space preservation. These areas will allow a variety of uses and residential densities and building types (consistent with the underlying recommended land use). These areas will provide additional open space and recreational amenities for residents, preserve environmentally sensitive resources as well as reduce stormwater runoff and water pollutants. This land use classification will correspond with the Conservation Development option for “R” Districts within the new zoning ordinance.

**Institutional:** Areas designated as Institutional include a variety of public and quasi-public uses and facilities including but not limited to: schools, churches, and public facilities that are government owned. More intensive uses like hospitals, and large government office buildings should be limited to appropriate non-residential areas.

**Light Industrial:** Primarily intended for industrial uses that might include light manufacturing, warehousing, wholesale storage, distribution centers, office parks and will allow on-site customers and other less intensive industrial uses. These areas are intended to promote the economic viability of industrial uses; encourage employment growth; and limit the encroachment of unplanned residential and other non-industrial development into industrial areas. This land use classification will correspond with the MI zoning category within the new zoning ordinance.

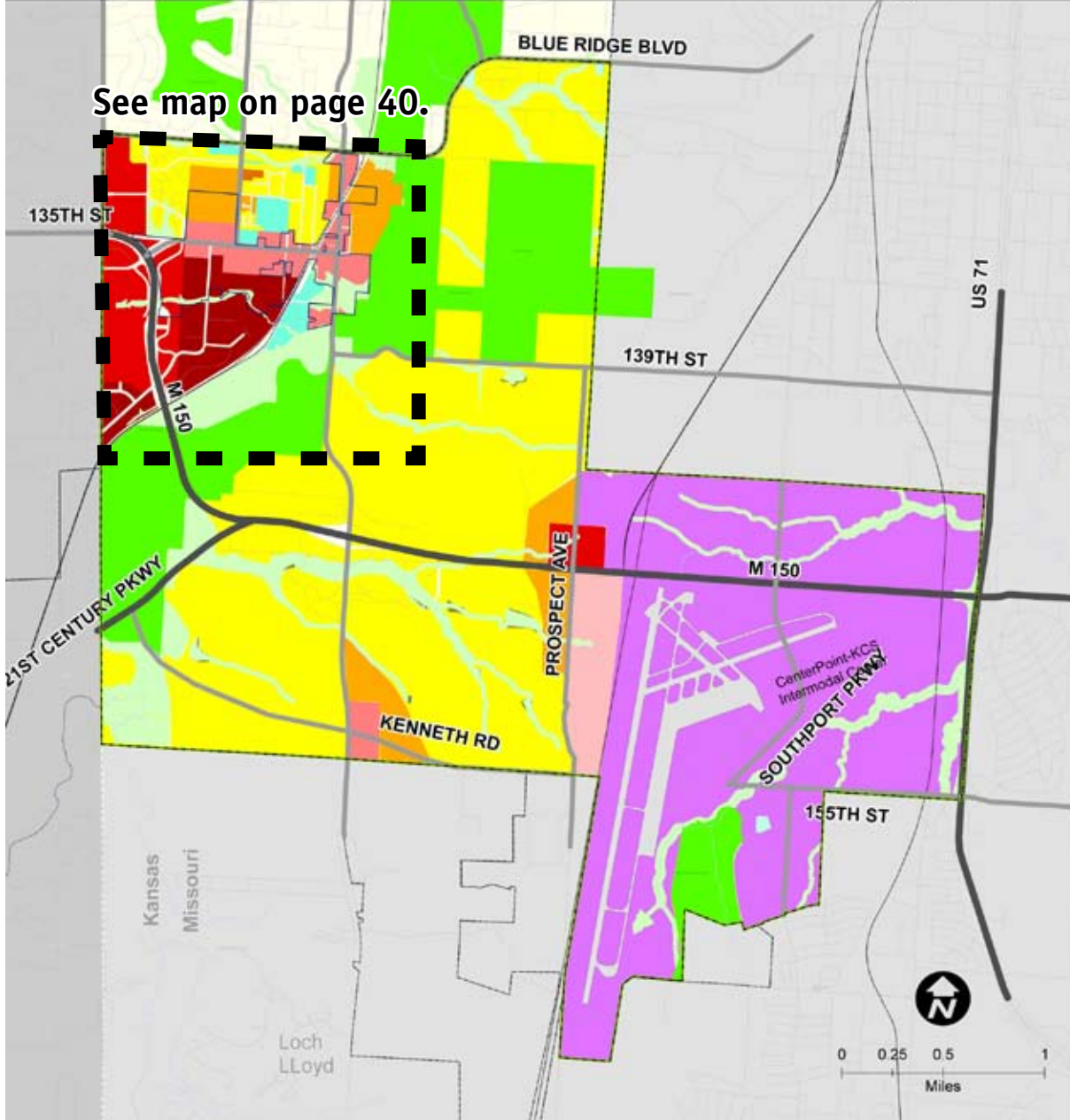
**Mixed-Use Neighborhood:** Primarily intended to accommodate and promote neighborhood serving retail sales or service uses, as well as mixed-use development consisting of businesses used on a building’s lower floors and residential uses on upper floors. This type of vertical, mixed-use development that includes a variety of business and residential choices should enhance the pedestrian environment of the community. Encouraging residential development in mixed-use areas provides increased housing choice and promotes higher density housing. This land use classification will correspond with the B1 and B2 zoning category within the new zoning ordinance.

**Mixed-Use Community:** Primarily intended to accommodate and promote a variety of community-serving retail sales or service uses generally of a higher intensity and larger scale than what is allowed in Mixed-Use Neighborhood areas. This category should include a mix of business and residential uses designed to enhance the pedestrian environment of the community and will correspond with the B3 zoning category within the new zoning ordinance.



## Land Use Plan






Martin City Area Plan

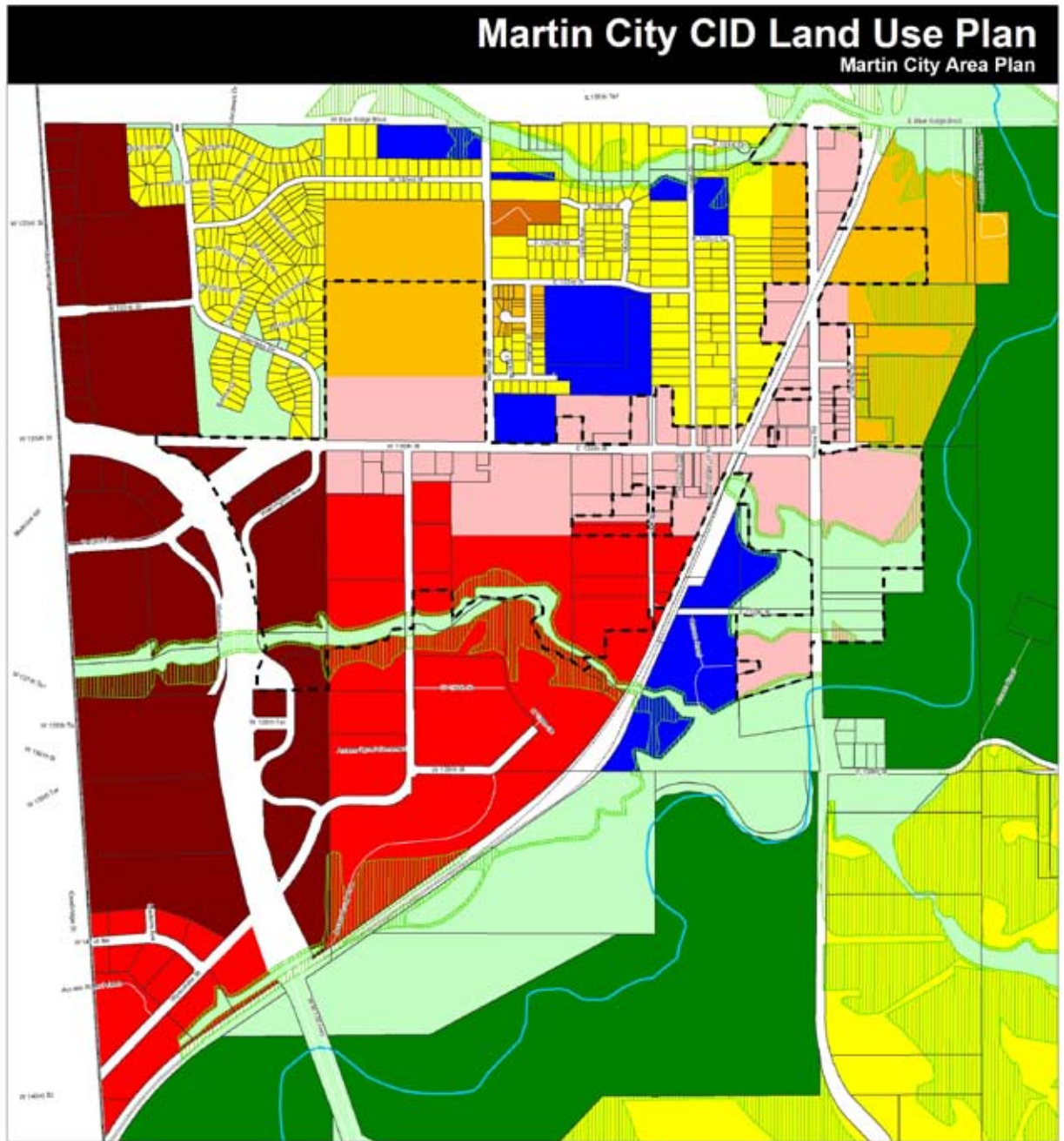


### Land Use Categories

 Commercial	 Open Space
 Institutional	 Park
 Light Industrial	 Residential Low Density
 Mixed Use Community	 Residential Medium Density
 Mixed Use Neighborhood	 Residential Medium-High Density
 Office	 Conservation Development

### Major Streets

 Arterial	 Community Improvement District
 Expressway	 City Limit
 Collector	

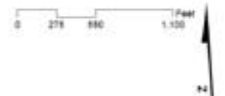


**Legend**

- KCMO City Limits
- Parcels
- Street Centerline (white line)
- Blue River
- Railroad
- Martin City CID (Boundaries current as of June 2008)

**Future Land Use**

- Residential Low Density
- Residential Medium Density
- Residential Medium-High Density
- Mixed Use Neighborhood
- Mixed Use Community
- Commercial
- Institutional
- Park
- Open Space / Buffer
- Conservation District





## Infrastructure Improvements

Infrastructure improvements, such as transportation and utilities, are essential components in the implementation of this plan. Transportation improvements address a wide-range of modes including roadways, transit, bikeways, trails, and railroads. Public utilities include both sewer and water.

All infrastructure should be phased in a logical manner to encourage contiguous and logical development and an efficient use of resources as mandated in the *FOCUS Kansas City Plan*

## Transportation

The *FOCUS Kansas City Plan* calls for the creation of an integrated, multi-modal transportation system. As an emerging area of Kansas City, the Martin City planning area has not yet developed a fully integrated transportation infrastructure. While the area is well served by freight rail and major highways, arterials, transit, bikeways and pedestrian amenities are deficient.

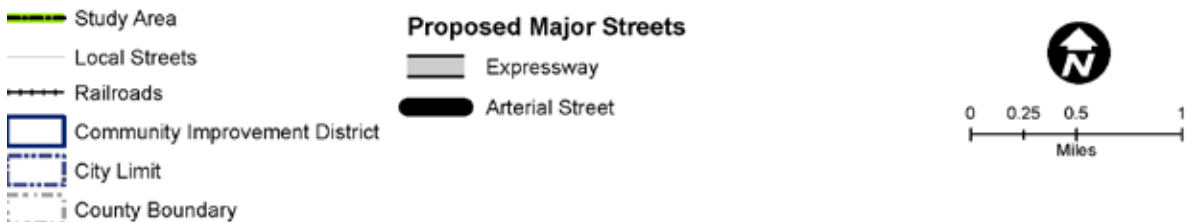
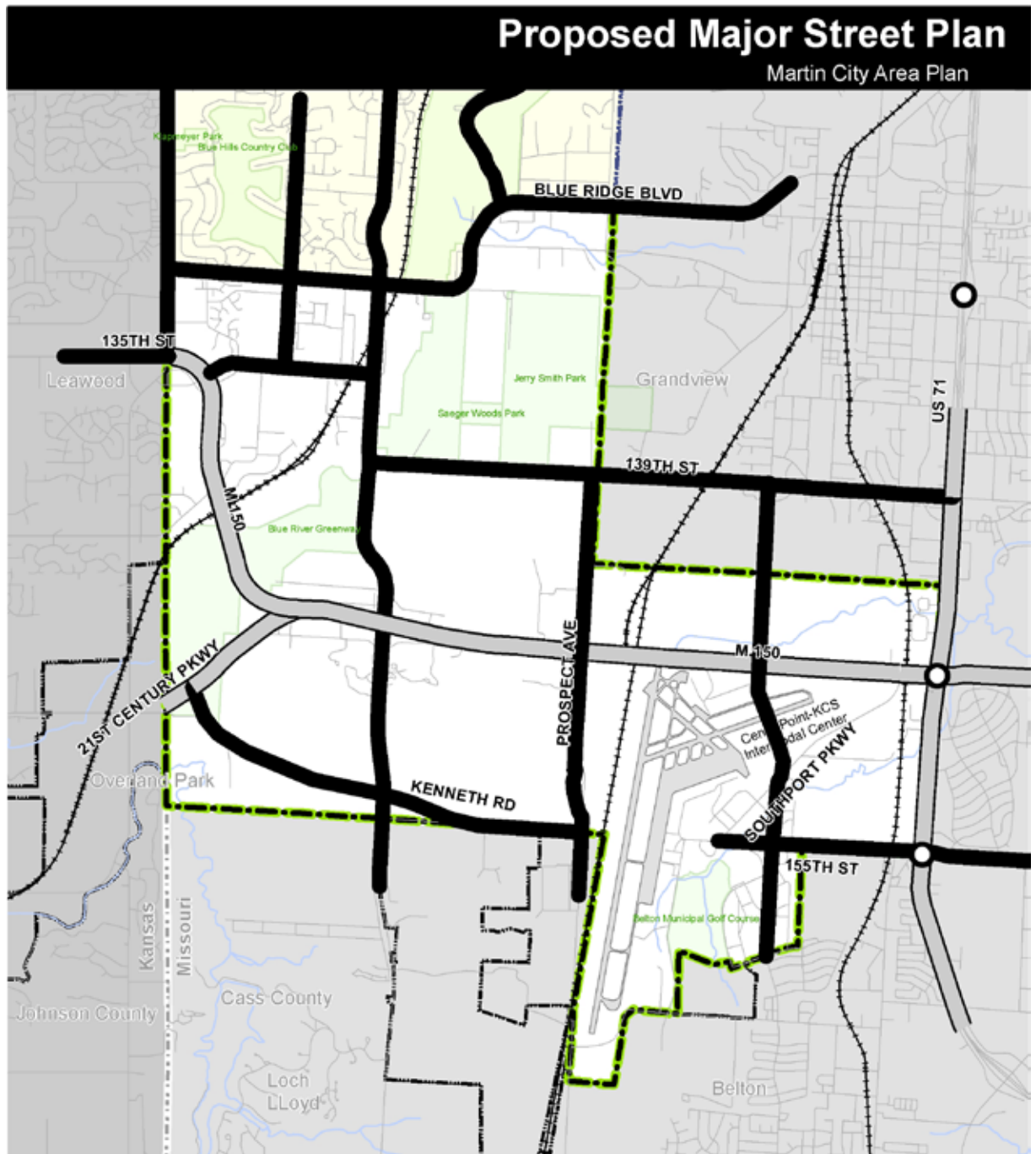
Recommendations for the creation of a multi-modal transportation system for the planning area include:

### **TRANSIT**

Express bus service by the Kansas City Area Transportation Authority currently runs along US Highway 71. As development occurs in the area, the feasibility of extending transit service into the new industrial development along Missouri I50 Highway and to mixed-use and commercial areas should be considered.

Mixed Use areas should support Transit Oriented Development (TOD). TODs encourage a mix of uses and design that compliment and encourage public transportation. Each of the mixed use nodes identified on the Land Use Plan are recommended to be developed in this manner.





## ROADWAYS

The development of a phased, extensive, primary arterial network is essential to support the proposed development within the plan area. This should include the construction of a system of collector and arterial streets as development occurs. The Proposed Major Street Plan illustrates the recommended future network of major streets.

These proposed changes should be included in the City's Major Street Plan and, ultimately, be incorporated in the Capital Improvements Program (CIP).

- Implement a system of collector streets as development occurs (constructed by the developer) to relieve pressure on arterial streets and ensure adequate circulation and connectivity. A collector street connection should be provided approximately every 1/4 to 1/3 mile. Collector Streets should connect from one development to another.
- Plan for and build an "economical" road system to meet existing and future needs.

Table xxxx

Street	Classification	Issues
US 71	Freeway	Evaluate capacity of US 71/M-150 intersection
M-150	Expressway	4 lanes / divided
Blue Ridge Blvd.	Primary Arterial	4 lanes / existing
Wornall Road	Secondary Arterial	2 lanes / existing
135th Street	Secondary Arterial	3 lanes / improved
139th Street	Primary Arterial	4 lanes / improved alignment near Blue River
Prospect Avenue	Primary Arterial	4 lanes / improved alignment near County Line Road
Holmes Road	Primary Arterial	4 lanes / improved Blue River Bridge and rail underpass improvements
Botts Road	Secondary Arterial	4 lanes / improved connection to Grandview & 155th Street
Kenneth Road	Primary Arterial	4 lanes / improved Blue River Bridge improvements
County Line Road	Primary Arterial	4 lanes / improved
Kensington Avenue	Primary Arterial	4 lanes / divided, improved and realigned
155th Street	Primary Arterial	4 lanes / divided, improved



# RECOMMENDATIONS

Future development in the MCCID area should be supported by the construction of future street connections, which will be a combination of arterial, collector, and local streets. These street connections are intended to benefit the area by providing enhanced connectivity and overall circulation, directing heavier commercial/industrial traffic away from 135th Street to controlled access points along M-150 Highway, and providing more controlled access opportunities along 135th Street. As development or redevelopment occurs, the future street connections should be constructed by the developer.





## Martin City CID Future Streets

Martin City Area Plan



### Legend

- ■ ■ Future Streets
- Parcels
- +— Railroad
- Martin City CID Boundary  
(Boundaries are current as of June 2008)

0 245 490 980 Feet





## TRAILS AND BIKEWAYS:

The following map depicts the proposed trail and bike route network in the area. This map is based on the approved bike route network, the Citywide Trails Plan and through an analysis of existing conditions. The following are recommendations from this plan:

- A system of on-street and off-street bicycle and multi-use trails should be provided to balance transportation and recreation needs (see Proposed Trails and Bike Routes Map).
- A “trailhead” should be developed near 135<sup>th</sup> Street and Holmes Road to access the proposed Blue River Trail.
- New development should incorporate “neighborhood” connections to the proposed regional trail system (Blue River Greenway and M-150 Trail) as development occurs. These local trails should also connect neighborhoods to parks, schools, churches and retail areas.
- All trails within the plan area should include enhanced pedestrian amenities including way finding signage, lighting, benches, trash receptacles, and generous landscaping.
- Proposed changes to the bike route network should be included in the City’s Major Street Plan.



## PEDESTRIAN

This plan recommends the development of compact, pedestrian intensive areas in all mixed-use areas. Sidewalks on both sides of the street should be provided in all new development. Additionally, where sidewalks do not occur in existing developments, efforts should be made to construct them.

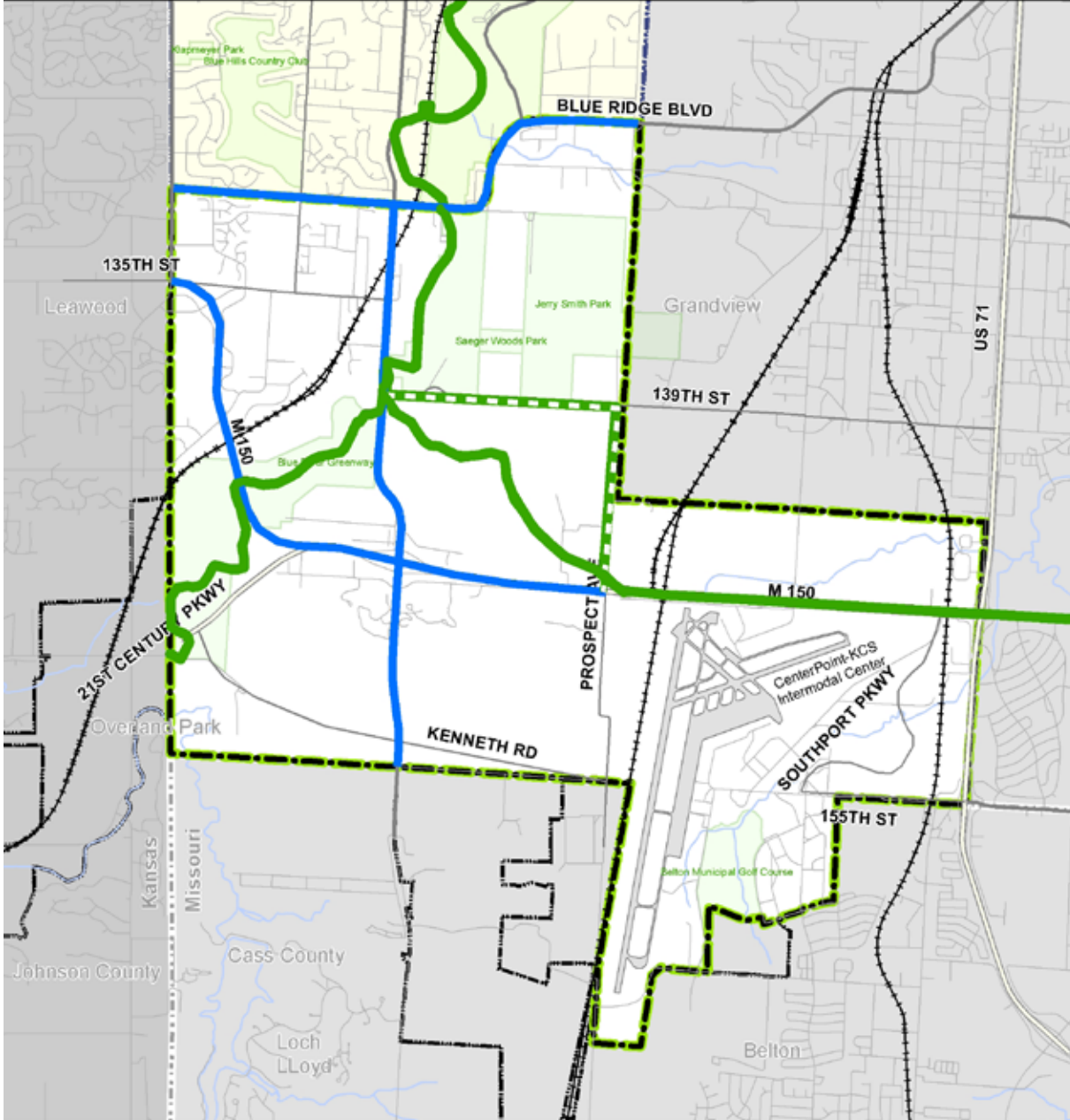
In order to accomplish improved walkability it is recommended that existing neighborhoods work with the City Planning Department to conduct a “walkability self assessment” to gauge existing conditions and make future recommended improvements. Neighborhoods should also partner with the Grandview School District to conduct Safe Routes to School studies around existing schools in the planning area.

All new developments projects should meet or exceed the minimum Level of Service recommendations and development guidelines in the City’s Walkability Plan. Development in the “Core District” in the MCCID area should meet minimum LOS for “Pedestrian Zones”.

A Pedestrian Oriented Overlay District (P/O) or a similar regulatory approach is recommended in the MCCID Master Planning Area generally located at 135<sup>th</sup> Street and Holmes Road. These regulations are intended to preserve and enhance the character of a pedestrian environment along 135<sup>th</sup> Street, Holmes Road, and connecting local streets to promote street-level activity, economic vitality, and pedestrian safety and comfort.

## Bicycle Routes and Citywide Trails Plan

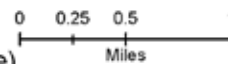
Martin City Area Plan



- Study Area
- Local Streets
- Railroads
- Community Improvement District
- City Limit
- County Boundary

### Bicycle Routes and Citywide Trails

- Bicycle Route per Major Street Plan
- Citywide Trails Plan (Draft)
- Citywide Trails Plan (Alternate Route)



# RECOMMENDATIONS

## **RAIL**

The area is well served by freight rail lines operated by Kansas City Southern and Union Pacific. The Centerpoint Intermodal Facility at the former Richards-Gebaur Airport is a significant outcome of this rail service. Rail traffic, however, when not adequately separated, can sometimes be disruptive to automobile traffic and adjacent communities.

Except in the Martin City CID area where the railroad crossing is considered an integral part of that district's character (see specific recommendations for the Martin City CID area).

This plan recommends that an engineering study be performed to re-evaluate proposed improvements on Holmes Road, including the need for a grade separated railroad crossing.

## **UTILITIES**

Key public utilities, such as water and sewer service, are instrumental to providing a favorable condition for development. Portions of the planning area are currently served by utilities, while other areas are not well served.

Of prime importance is sanitary sewer service. This service is currently located in the northern portion of the planning area and follows the Blue River Corridor. The southwestern portion of the area is not currently served by sewer and, because service requires crossing the Blue River, it is more difficult and expensive.

Centerpoint Intermodal Facility is potentially served by a trunk sewer line to the east of US 71. Ultimately, this plan contemplates the extension of both sewer trunk lines further into the planning area.

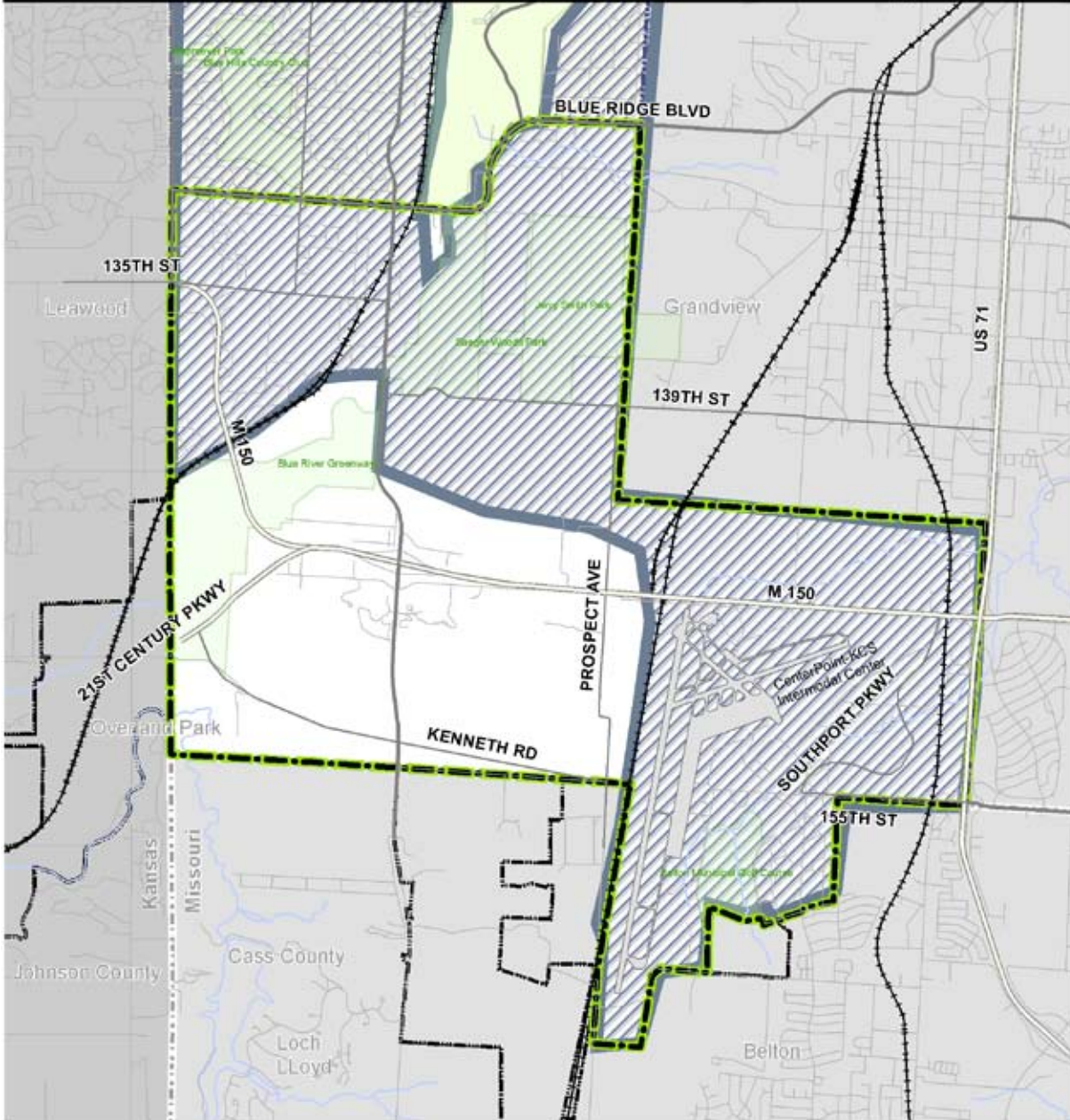
The FOCUS Kansas City Plan seeks logical extensions of both development and accompanying sewer service. The Development Priority Zones Map (darker tone) indicates areas that are more easily served.


This suggests a long-range phasing strategy for development and infrastructure that should be followed in implementing this plan.



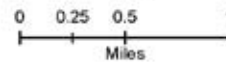
## Development Priority Zones

Martin City Area Plan



-  Study Area
-  Local Streets
-  Railroads
-  Community Improvement District
-  City Limit
-  County Boundary

 Development Priority Zones



# RECOMMENDATIONS

## Natural Resources

The planning area is blessed with abundant natural features through the Blue River Corridor – its rolling terrain, woodlands, and dedicated parklands.

This plan recommends the maintenance and enhancement of these natural features through the following actions:

1. The extension of dedicated parks and greenways into the area as property develops, either through public actions or as a condition for private development approval.
2. The development of a trails system that links the entire district. This system should accommodate pedestrians, bicyclists, and recreational modes, including equestrian where appropriate.
3. The sensitive integration of development in harmony with both the natural terrain and wooded areas. As much as is practical, these features should be integral to development proposals.
4. The recognition of and preservation of scenic views and vistas associated with the Blue River Corridor.

## Urban Design Guidelines

### INTRODUCTION

The physical appearance of development is a major factor in determining public perceptions of an area. Throughout the plan process, area stakeholders expressed a desire for new development to create a sense of place and enhance the area's existing small town/rural character. The following Design Guidelines provide a framework for quality development consistent with the plan vision.

### FOCUS KANSAS CITY PLAN - QUALITY PLACES GUIDELINES

These 'Quality Places Guidelines' will act as the basic framework from which to create high quality places to live and work. These characteristics will be achieved by implementing the more specific guidelines in this chapter.

- Walking is feasible, safe, and inviting.
- Streets form a continuous network, are of minimum width, are well designed, and allow for adequate public safety and traffic volume.
- Transportation alternatives are convenient and easy to use.
- Existing "historic buildings" are refurbished and reused.
- Buildings are designed to create or contribute to a sense of community.
- Residential land use is an essential component of mixed-use development.
- Neighborhoods and commercial districts have identifiable centers that create places for residents or employees to gather, interact, and communicate, and that help create an identity for the area.
- New development occurs at infill sites or contiguous to existing development and uses existing infrastructure effectively.
- Neighborhoods and districts have distinct and identifiable characters.
- Development preserves or creates open space, respects existing topography, and minimizes the impact of development on the natural environment.
- New development and infrastructure are built to be useful for 100 years or more.

## LANDSCAPE, SCREENING, AND TRANSITIONS

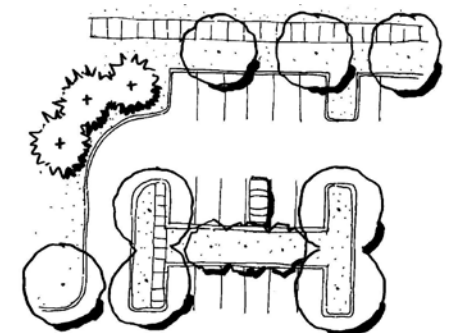
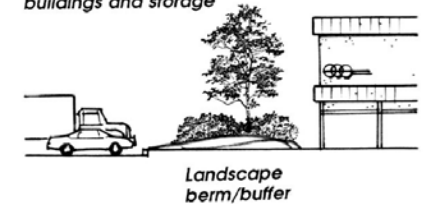
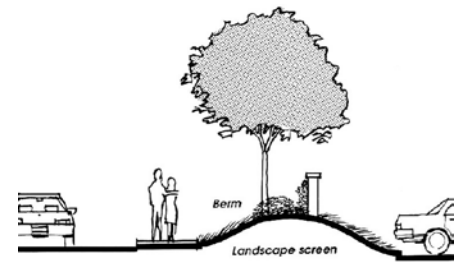
### INTENT:

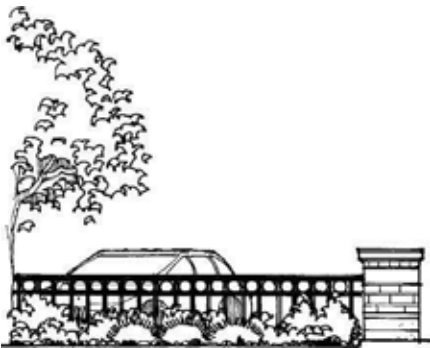
- To provide a balance between buildings, impervious surfaces, and landscaped areas through enhancement of streets, parking lots, plazas, open space, buildings, gateways, and other structures.
- To provide screening/buffering between structures, parking lots, pedestrian paths and between developments of varying intensity and scale.
- To provide a physical and visual separation between higher and lower intensity uses by softening and mitigating the impacts of large buildings and paved areas.

### LANDSCAPE AND SCREENING GUIDELINES:

All development and redevelopment proposals should provide a landscape plan prepared by a qualified design professional, which incorporates the following guidelines.

- Screen all trash dumpsters, storage areas, service areas and loading areas with a combination of landscaping, decorative walls/fences or berms.
- Landscape the interior and perimeter of all parking lots. Screen surface parking lots adjacent to streets with a combination of landscaping, decorative walls/fences and berms at least 4 feet in height.
- Building foundations shall be planted with ornamental plant material, such as ornamental trees, flowering shrubs and perennials, and ground covers.
- Guidelines for walls and fences:
  - Fences that face primary street entrances should generally be between 18" to 42."
  - Walls and fences shall be constructed of high quality materials, such as decorative blocks, brick, stone, and wrought iron.
  - Walls and fences should be complemented with landscaping.
  - Where chain link or security fencing is required, landscaping should be used to screen such fencing from view from adjoining streets and development. Plastic slats should not be used as an alternative.
- Use landscaping to define and enhance the sense of arrival at appropriate site entries, and to visually frame buildings.
- Plant materials shall be suited to a suburban environment and local climate. Native plant materials are encouraged. A mix of evergreen and/or deciduous plant material should be used.
- Raised planters and/or planting beds should be used adjacent to roadways where landscape is vulnerable to water splash from passing vehicles.
- Alternative storm water solutions should be considered in the design / construction phase, examples include: storm water inlet alternatives, rain gardens and drought tolerant plants.
- New development should provide street trees of a size, spacing, and type to be approved by the City Forester. In general, one street tree is required for every forty (40) feet of lineal frontage.
- All new development should attempt to achieve harmony with the natural environment by preserving existing, healthy, attractive plant materials of significant size.





## TRANSITION GUIDELINES:

- Transitions should be provided between changes in use or intensity of use.
- Dissimilar or incompatible uses should be separated by a major street when possible. However, when such uses are located adjacent to one another, the transition/screening techniques described below should be used.
- Architectural Transitions and Green/Open Space Transitions should be the primary transition technique between uses.

### Architectural Transitions include:

1. Use similar building setbacks, height and roof forms.
2. Mitigate the larger mass of buildings with facade articulation
3. Reduce building heights, intensity of use and densities as development moves closer to low intensity areas

### Green/Open Space Transitions include:

1. Small green spaces, courts, squares, parks, or plazas
  2. Existing natural features, including changes in topography (not retaining walls), streams, existing stands of trees, etc.
- A combination of landscaping, walls, fences and/or berms should be used where other transitions tools are not possible, or where other transition tools are not adequate.
  - Connections between developments should be incorporated into transition design.
  - Provide landscape transitions between developed and natural areas.





## OPEN SPACE

### INTENT:

- To provide well defined natural and developed open spaces as amenities that serve as the focus of block, lot, and circulation patterns.
- To supplement public open space with privately developed open space that completes linkages.

### OPEN SPACE GUIDELINES:

- All areas not occupied by buildings, structures, streets, driveways and parking lots should be identified as “public spaces.”
- Open spaces must be integrated into the development design, and not placed on undevelopable remnant or unusable perimeter buffers.
- Provide small developed open spaces in new development (i.e. a neighborhood park in residential areas or public plaza in commercial areas).
- All open space areas must be accessible from a public street, sidewalk or trail.
- Set back buildings, parking, and grading from significant natural features to ensure their continued quality and natural functions.
- Open space areas should be visible, safe, attractive and inviting by incorporating pedestrian lighting, public art, landscaping, benches, and other amenities.

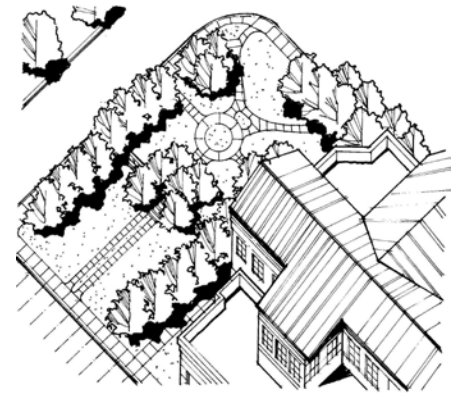
## ENVIRONMENTAL AND STORMWATER MANAGEMENT

### INTENT:

- To protect the existing environmental assets of the area through increased storm water infiltration, reducing flooding and improving water quality.

### ENVIRONMENTAL AND STORMWATER MANAGEMENT GUIDELINES:

- Retain the natural and visual character derived from topography, woodlands, streams, and riparian corridors. Hills and natural slopes should be preserved and excessive cuts and fills should be avoided.
- Provide greenway corridors to preserve natural drainage areas, floodplains, slopes over 15 percent and wooded areas.
- Provide a tree survey and preservation plan with the development application. Replace trees over 10-inches in caliper to be removed by construction.
- Views of rivers and natural features should be preserved and integrated into developments. Complete removal of trees to create views is discouraged.
- Provide detention and use non-structural stormwater BMPs to preserve open space within and between developments, and provide storm water treatment. Design stormwater management areas as attractive water feature amenities or focal points.
- Limit storm water runoff from new developments to predevelopment levels.
- Green roofs and pervious pavers and other techniques to reduce runoff and increase absorption are encouraged.
- In residential areas, allow alternative local and collector street designs with vegetated swales in lieu of enclosed storm water systems.



# RECOMMENDATIONS

## PEDESTRIAN AND BICYCLE CIRCULATION

### INTENT:

- Meet minimum level of service as recommended in the Kansas City Walkability Plan.
- To provide a safe, direct, continuous, convenient, inter-connected, and visually pleasing system of pedestrian walks, trails, and bike routes.
- To provide a pedestrian/bicycle network with the same or higher priority as vehicular traffic.



### PEDESTRIAN AND BICYCLE GUIDELINES

- Projects abutting the Blue River Trail or a regional trail corridor should incorporate pedestrian and bike connections to these amenities.
- Provide pedestrian walkways/sidewalks which connect:
  - ✓ The primary building entry to the street sidewalk by the most direct route.
  - ✓ All buildings, open space and parking areas within a development and link to adjacent streets, development and open space systems.
  - ✓ All internal streets/drives to sidewalks along perimeter streets.
- Extend walkways through all parking areas within linear landscape strips to define pedestrian paths.
- Where a walkway crosses a street, drive-aisle or driveway, it should be clearly delineated by a change in paving materials, color, texture, or height.
- Provide pedestrian and bicycle connections where automobile connections are not feasible.
- Provide on-site bicycle parking areas in visible, active, well lit areas near building entries.
- Minimize street crossing distances.
- Set back sidewalks from the street and include a lawn for trees between the curb and the sidewalk. Allow sidewalks closer to the street curb in commercial or mixed-use areas and incorporate tree planters and landscape when sidewalks are adjacent to the curb.



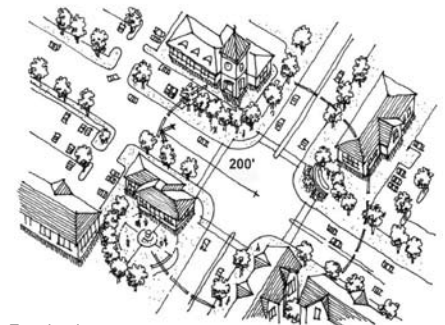
## SITE PLANNING

### INTENT:

- To utilize building placement and open space to provide for compatibility of use, access, and circulation between adjoining properties.
- To provide special definition of streets at key locations such as arterial street intersections or area gateways and maximize the positive character of streets and buildings through continuity of architecture and landscape frontage.

### SITE PLANNING GUIDELINES (ALL RESIDENTIAL, COMMERCIAL AND MIXED-USE DISTRICTS)

- Access to commercial and industrial uses should only be from major highways, arterials, or commercial/industrial collector streets. There should be no through truck access to residential areas from industrial streets.
- The rural character of the area should be maintained by preserving existing wooded areas, using extensive landscaping and minimizing curb cuts.
- Streets should form a network of regular intersections and connect neighborhoods. Continue streets through to as many neighborhoods as possible and allow for future connections where topography permits. New development should incorporate a system of collector streets, with a collector street connection approximately every 1/3 to 1/4 mile. New developments should connect to streets in adjacent developments.
- Streets should follow natural contours to minimize the impact on the natural terrain.
- Streets should be the minimum width practicable and should accommodate pedestrians, bicyclists, and automobiles.
- Avoid cul-de-sacs when through street connections are not desirable. Provide streets parallel to open space or looped streets with neighborhood greens.
- Locate parking, service areas and vehicular circulation behind or to the side of buildings and not along primary street frontage(s).
- Front buildings onto a street or major access drive to create a clear street edge and to provide physical definition of roadways.
- Corners of major intersections should include a “focal point” within a 200-foot radius of the center of the intersection and around “gateway” areas. Focal points should include vertical architectural features, fountains, public art, and/or public plazas.
- Parking areas should not be located within a 200-foot radius of the center point of a major street intersection or gateway, unless located behind a building.
- Only one curb cut will be allowed for each development adjacent to an arterial or parkway, except for projects that have recommendations related to a professional traffic study.



Focal point

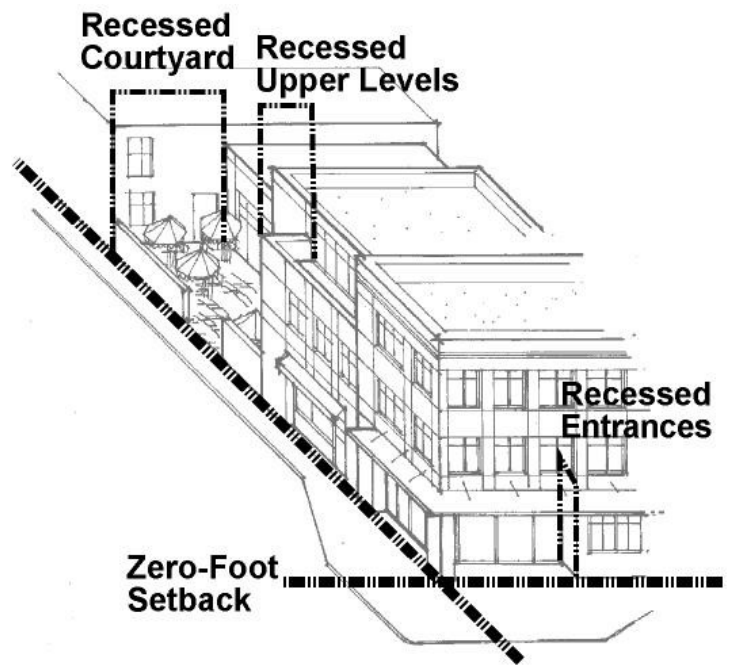
# RECOMMENDATIONS

## RESIDENTIAL SITE PLANNING GUIDELINES

- Provide rear alley/rear garage access for attached residential structures in medium density residential areas and mixed use areas.
- Avoid direct driveway access on major streets for residential properties, except for residential estate lots.

## COMMERCIAL AND MIXED-USE SITE PLANNING GUIDELINES

- Mixed-Use Districts will have a unique character or sense of place with an identifiable center that includes defined “people places” for residents, shoppers, workers and visitors to gather, interact and recreate and provide opportunities for housing choice and variety.
- Mixed-use districts will develop as multi-modal hubs. Development densities for commercial, office and residential uses will be greater within these areas to encourage pedestrian activity and transit use.
- Provide a tight network of bicycle and pedestrian friendly streets, wide sidewalks, benches, street trees and landscaping, and on-street parking.
- Locate buildings in commercial/mixed-use areas along a build-to-line with parking located predominately behind buildings. Limit the amount of parking and vehicular circulation located between the building and the street.
- Nonresidential freestanding buildings should be clustered to define the street edge and create plazas or public gathering spaces between buildings.
- Frame and enclose parking areas with buildings on at least three sides. A majority of the frontage along adjacent streets should be occupied by buildings, decorative architectural walls or landscaping.
- Increase sidewalk width when adjacent to on-street parking and include a “transition zone” of pedestrian amenities along the street including street trees, landscape planters, pedestrian lighting, and other streetscape amenities.





## ARCHITECTURAL CHARACTER

### INTENT:

- To create a built environment that is in scale and character with pedestrian activities and to ensure high quality appearance, form, and scale of buildings to enhance the character of the area and provide long term value.

### ARCHITECTURAL CHARACTER GUIDELINES (ALL DISTRICTS)

- Architectural materials should compliment the character of the existing built environment through use of high quality, durable materials. Suggested materials include: wood; masonry; limited concrete; stone; cast stone; and tile.
- Materials, such as imitation masonry, metal panels, vinyl siding, concrete panels, or plywood, are discouraged on commercial buildings.
- Buildings within a development should have a coherent architectural theme in terms of mass, height, rooflines, and materials.
- Buildings facing major streets should integrate architectural details to enhance the street edge and promote human scale and interest. Suggested materials and details include but are not limited to corner elements, awnings, window inlets, planted window boxes and articulated entries.
- Green (LEED certified) architecture is encouraged for new large scale commercial or mixed-use development.
- Use building placement and design to define roadways as civic spaces.
- Design buildings to relate directly to the street using the following techniques:
  - ✓ All building frontages visible from a street or a residential area shall have the equivalent treatment of the primary building façade.
  - ✓ Provide a primary entry for building facades facing arterial streets, or a facade treatment of comparable architectural, material, and detailing quality.
  - ✓ Minimize long expanses of wall at a single height or in a single plane.
  - ✓ Vary floor heights to follow natural grade contours if significant variation is present.
  - ✓ Use the highest architectural detailing when located near a major intersection or primary image street.
- Design buildings to provide human scale, interest, and variety using the following techniques:
  - ✓ Use the highest level of architectural detail near streets and entries, and around the ground floor.
  - ✓ Vary building massing, height, profile, and roof form that provide human scale while maintaining a consistent overall building form to the street edge.
  - ✓ Vary building form with recessed or projecting bays and changes in materials, details, surface relief, color, and texture.
  - ✓ Expression of architectural or structural modules and detail.
  - ✓ Diversity of window size, shape, or patterns that relate to interior functions.
- Provide complementary variations in building form, unless the area is designed in a manner that relies on uniformity to establish an architecturally pleasing pattern.
- Provide windows, doors, plazas, and other features on building facades adjacent to open space to encourage pedestrian activity and provide visual oversight.

# RECOMMENDATIONS

## RESIDENTIAL ARCHITECTURAL CHARACTER GUIDELINES

- Residential developments shall avoid monotonous rows of garage doors visible from the street.
- Provide residential dwelling designs with alternatives to street oriented garages, such as a mixture of rear and side loaded garages, attached and detached garages, carports, and porte cocheres.

## COMMERCIAL AND MIXED-USE ARCHITECTURAL GUIDELINES

- Primary building facades should be parallel to the sidewalk. Buildings should define a majority of the street edge. Surface parking lots are encouraged between or behind buildings.
- Where buildings are set back from the sidewalk, such areas should be treated as public spaces such as a plaza or courtyard.
- The front of all mixed-use buildings should include pedestrian-oriented elements such as: transparent display windows; outdoor seating for dining areas; public art and pedestrian amenities such as fountains and benches.
- Locate and design large buildings to minimize windowless walls and service areas visible from public streets.
- Provide ground floor retail with direct pedestrian entries oriented toward public streets, parks, or plazas. Primary entries must be easily accessed and directly visible from a street. Entries should be marked by architectural features such as overhangs, special lighting, awnings and/or signage.
- Include a repeating pattern on building facades that includes color change, texture change and material change, with at least one of the elements repeating horizontally.
- Outside sales, storage, or display areas are discouraged. When permitted, such areas shall be screened with landscaping or enclosed with materials integral to the building architecture.
- Provide a clear and consistent street edge with at least 50% of the building's "active wall" oriented toward the street. An "active wall" is the side of the building containing the majority of the storefronts, customer entrances, and windows.
- Provide no less than 20% window to solid wall area for portions of a building facade above the ground floor.
- Street level uses should have a transparent quality. Sidewalk traffic as well as passing vehicles should be able to see activity within the building.
- Incorporate transparent glazing at all occupied levels of building facades oriented toward streets and pedestrian areas.
- Provide arcades, display windows, entry areas, awnings, and other features along no less than 60% of the ground floor facing public streets.
- Roof form, material, color, trim, and lighting should be an integral part of the building architecture. Roofs should not serve as attention-getting devices for signage or as an identifiable corporate image.
- Locate drive-through facilities, when permitted, on the side or rear of a building away from a street.

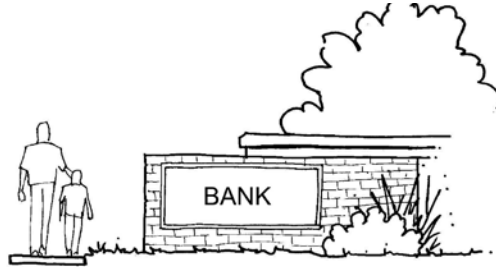
## SIGNS

### INTENT:

- To provide clear, simple, easily understandable, coordinated method of conveying information for businesses and address identification.
- To ensure that signage is unobtrusive and integrated with the buildings and/or landscape design.

### SIGN GUIDELINES

- Signs should be visible and clearly legible for both the pedestrian and motorist.
- Signs should be highly graphic with a minimum number of words.
- Freestanding pole/pylon signs are not allowed within new developments. Monument signs are encouraged.
- Signs must not obscure important architectural features.
- Moving or revolving signs and flashing signs are discouraged.
- Do not approve new off-site advertising signs and remove existing non-conforming signs whenever legally possible. Remove existing billboards if public funds are used for a project. The practice of substitution and/or relocation of billboard signs is discouraged.
- Monument signs should be landscaped to complement the existing or proposed landscaping of the project.
- Non-standard sign forms are encouraged.
- Signs should be consistent with the design, materials and colors of the overall development.
- Signs should be made of high quality and durable materials such as brick, stone, or metal.
- When lighting is used to illuminate signs, it should be designed to eliminate glare and spill over onto adjacent properties



# RECOMMENDATIONS

## MARTIN CITY CID GUIDELINES

The Martin City CID Core Area design guidelines are intended to supplement the Area Plan Site Planning and Mixed-Use Planning Guidelines. These additional guidelines further define and enhance the character of the core area pedestrian mixed-use environment with complimentary compact and small-scale uses in the Core area. The identity of the Martin City Core is based on the following:

- Small-scale businesses emphasizing specialty goods and services creating a unique character and sense of place.
- Residential uses well integrated in close proximity to retail.
- Business character and building appearance unique to Martin City, not including prototypical corporate design.
- A built environment organized in a manner compatible with a vibrant, walkable village cluster.
- This area should be established as a “Pedestrian Street” designation.

## ADDITIONAL SITE ORIENTATION GUIDELINES FOR THE CID CORE AREA

- A minimum of 75 percent of buildings should have a zero foot setback.
- Primary facades should be parallel to the sidewalk.
- Parking should be provided in the rear, and on-street. However parking may be located to the side of the building, provided it does not occupy more than 50 percent of the street frontage and it is screened by a brick wall, wrought-iron or decorative fence, or landscaping.

## ADDITIONAL CIVIC IDENTITY AND PEDESTRIAN REALM GUIDELINES FOR THE CID CORE AREA

Pedestrian level landscape and streetscape features should both beautify and provide identity signage for the Martin City area. District identity signage should be integrated into monument signs, banners, unique gateway markers, and streetscape improvements.



Focal point corners should consist of enhanced architecture and amenities, with off-street parking screened from view. Focal points should include exceptional architectural design, a vertical architectural feature, public art, and/or exceptional designed public plaza and landscape amenities.

- The 135th Street and Holmes Road intersection is designated as a primary “focal point” for site planning and architectural standards.
- Secondary focal points should be provided around the Holmes Road and Blue Ridge Blvd intersection, along Holmes Road near the 139th street intersection, and along 135th Street around the Oak Street intersection.



Gateway markers at with enhance landscape and streetscape features should be incorporated on both public and private property at key intersections and focal point corners to identify the entrances to the District and add to the area's sense of place.



Streetscape enhancements between the curb line and the building wall include amenities such as distinctive street lighting, landscaping, street furniture, intersection treatments, information signature, public art, and sidewalk design to enhance the pedestrian experience.



The street edge should be defined with streetscape elements such as plantings, low walls, screens and fences, public art treatments, and building walls.



Gathering places and open spaces should be integrated along the streetscape and between buildings. Where buildings or portions of buildings are set back from the sidewalk, such areas should be treated as public spaces such as a plaza or courtyard.



Accommodations for outdoor sidewalk seating should be provided wherever possible to enhance the pedestrian environment.



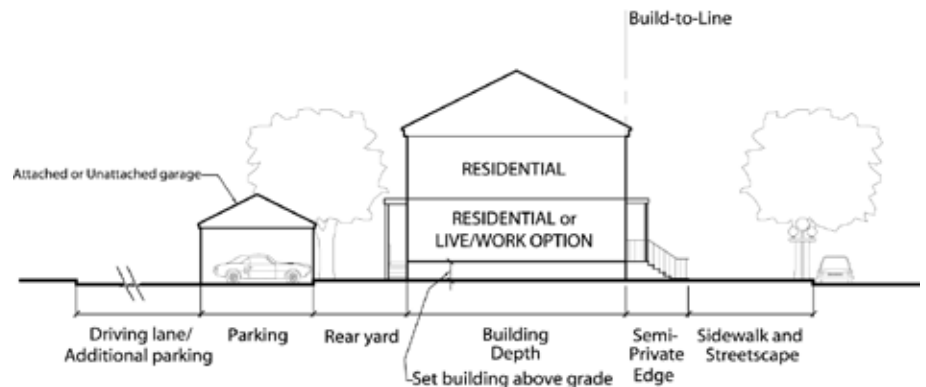
# RECOMMENDATIONS

## ADDITIONAL RESIDENTIAL GUIDELINES FOR THE MARTIN CITY CID CORE AREA (ATTACHED HOUSING, TOWNHOMES, CONDOS)

The Martin City Core area should include a range of housing options. Residential uses incorporated within the mixed-use areas along I35th Street and Holmes Road should conform to the following guidelines (See the Additional Commercial/Mixed Use Guidelines for structures with residential above-retail):

- **Building Height:** 2-4 stories
- Ground-level residential “live-work” options are encouraged.

### Conceptual Residential Building Type in a Mixed Use Area (Attached Housing, Townhomes, condos)



*Note: This conceptual residential building type is intended to illustrate the design guideline framework for new development to reinforce and enhance the Martin City mixed use core area. Other building types that conform to the design guidelines are encouraged.*



Front entrances are oriented toward the street and marked by architectural features.



Buildings should face the street to define the street edge, and integrate architectural details that promote human scale and enhance the pedestrian experience.



The design of buildings facing the street should contribute to the public realm; promote interaction with the pedestrian environment along the street, incorporating elements such as a front porch, stoop, portico, semi-private courtyard and similar features. Residential structures should be set above the sidewalk to provide some privacy from the sidewalk and the street.

# MARTIN CITY

New residential development should be compatible with the scale and character of surrounding established neighborhoods, as well as the design character of nearby commercial and mixed use buildings.

Parking should be located to the rear of the building, with no front-loaded garages oriented toward I35th Street or Holmes Road. Garages and parking areas should be located where not visible from adjacent properties, public spaces, or streets.

Unique architectural styles and exceptional architectural design are encouraged in high visibility locations and focal point corners.

Higher density housing in the mixed use core area should embrace the street edge and incorporate variation of massing configurations, such as a variety of materials and a variety of wall and roof planes to provide complexity and visual interest.

## ADDITIONAL COMMERCIAL AND MIXED USE GUIDELINES FOR THE MARTIN CITY CORE AREA

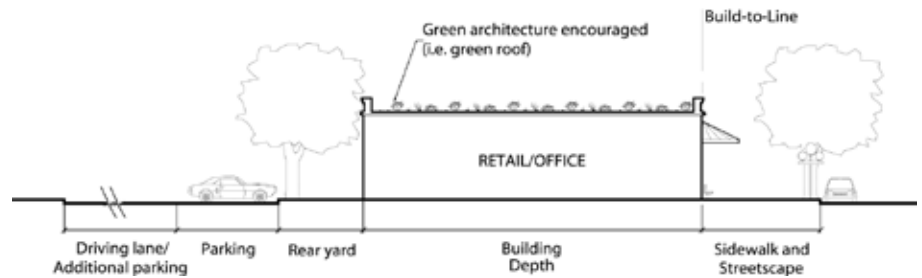
Buildings should be designed and oriented toward I35th Street and Holmes Road to encourage a pedestrian environment, with the highest intensity and tallest structures centered in the vicinity of the I35th Street/Holmes Road intersection.

- **Building Height:** 1-4 stories. Buildings at least 2 stories in height, or the appearance of more than one story, are preferred along both 135th Street and Holmes Road.
- **Building Massing:** Buildings should be designed to create the appearance of small-scale business establishments.
- **Building Materials:** Architectural materials should compliment the character of the existing built environment through use of high quality, durable materials. Suggested materials include: wood, masonry, concrete, stone, cast stone, and tile. The use of vinyl and metal siding should be limited to accent materials and not serve as the predominant exterior finish. The use of EIFS or stucco board should be accented with one or more of the suggested materials noted above.

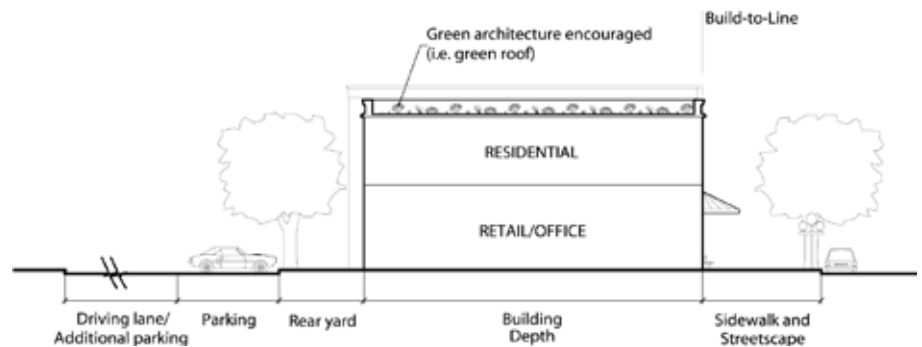


# RECOMMENDATIONS

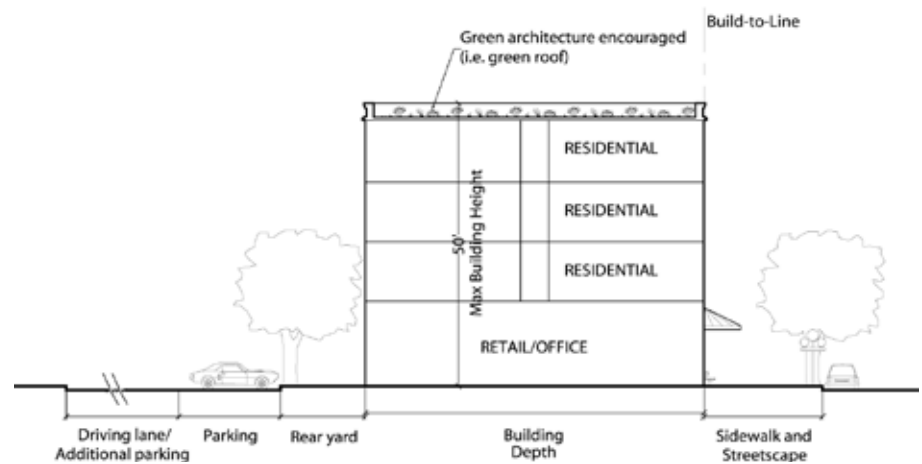
## Conceptual Single-Story Commercial Building Type



## Conceptual Multi-Story Commercial / Mixed-Use Building Type



## Conceptual Multi-Story Mixed-Use Building Type



*Note: The three conceptual building types are intended to illustrate the design guideline framework for new development and redevelopment that reinforces and enhances the Martin City mixed-use core area. Additional building types that conform to the design guidelines are encouraged.*



# MARTIN CITY

The existing building stock and **community character should be retained** and enhanced whenever possible.

Accommodations for **outdoor sidewalk seating** are encouraged for eating, drinking, and entertainment establishments. Such seating areas must maintain adequate sidewalk circulation for pedestrians.

Rehabilitation and renovation of existing buildings should **integrate with neighboring buildings** in terms of massing and proportion.

Buildings facades visible from the street should be treated as a “front” and designed to clearly **articulate a base, middle, and top**.

Buildings in close proximity to single-family residential should **include architectural transitions to ensure compatibility**, such as similar setbacks, height, sloped roof forms, and building materials.

**Residential above retail** and outdoor rooftop gathering spaces are encouraged to help humanize the building scale for pedestrians.





# RECOMMENDATIONS

Street facing building facades should integrate architectural details to enhance and promote **human scale and interest**, such as corner elements, awnings, windows and window inlets, planted window boxes and articulated entries.

Buildings at street corners should effectively address both streets of the intersection and provide human scale and exceptional **architectural design features** oriented toward the street.

Freestanding buildings or buildings with single tenants should be designed and located to **define a majority of the street edge**.

Large buildings should be designed to reduce their perceived height by **dividing the building mass into smaller scale components**.

Street level retail space should have a **transparent quality**. Sidewalk traffic as well as passing vehicles should be able to see activity within the building.

Street facing facades should have a workable **public entrance door facing the street** and incorporate a large expanse of glass for ground level transparency. Entries should be marked by architectural features such as projecting overhangs, special lighting, awnings and/or signage to signify the location and importance of entries.

# IMPLEMENTATION

Implementation is the process of putting the area plan into action. This happens through private development applications, city investments and improvements, partnerships serving as community catalysts to organize citizens and private funds, and local businesses starting, growing and changing. There are many tools in the implementation “toolbox”, each of which serves a slightly different purpose, yet are often connected thereby creating an integrated implementation program. These “tools” include, but are not limited to:

**Zoning:** The delineation of districts and the establishment of regulations governing the use, placement, spacing, and size of land and buildings. Zoning is intended as a method to guide overall development of an area, with general guidelines for specific lot design and construction.

**Subdivision Regulations:** Address the layout and placement of a new or enlarged development within a specific zone. Subdivision regulations are typically applicable to residential developments over a certain lot size (such as 3 or more lots), and are increasingly made applicable to non-residential development as well. These regulations are an appropriate tool for analyzing the design of new development and its impact on existing public facilities and services.

**Design Guidelines:** Establish a cohesive design pattern for all development within specific areas, such as mixed-use centers or residential neighborhoods. Design guidelines may address the following topics: site and environmental preservation; open space; buildings; streets; parking; and signage.

**Public Infrastructure Extension Policies:** Each new development has a measurable impact on the level of service of existing or proposed new public services and facilities. As part of the planning process, the city can incorporate required levels of service for specific facilities and services (water, sewer, stormwater, streets) and require that all new development ensure that adequate facilities exist to serve proposed new development.

# IMPLEMENTATION

Where infrastructure extension is necessary outside of the development priority zone, that developer is required to pay the cost of the required facilities and services.

**Impact Assessment:** An information gathering and decision making component which provides the decision maker with an objective basis for granting or denying approval for a proposed development. Typical impact review includes traffic impact, environmental impact, and fiscal impact.

**Site Design:** Usually focuses on landscaping and buffering which serve a critical function of buffering more intense uses from less intense uses, as well as adding beauty and interest to individual building sites. With a plan that blends commercial and retail uses in the mixed-use centers and provides for changes of density over the various residential, commercial, and industrial uses, it is necessary to ensure that different categories of use are buffered from each other. It is equally important to ensure that landscaping is integrated into all site development to reflect the area's character and provide for a blending of uses in the existing landscape.

**Capital Improvements Programming:** The process to prioritize and direct resource (funding) allocation for appropriate projects including streets, water, wastewater, drainage and parks.

**Growth Coordination:** A process which coordinates the timing, intensity, and density of growth with the availability of public facilities and services. The City has prepared an overall growth coordination framework that identifies a boundary for growth known as the development priority zone. This framework should include criteria for rezoning and subdivision approvals to be used by decision-makers when making determinations in outlying or fringe growth areas. The framework can be used to guide new development to appropriate growth areas while protecting limited development areas as identified in the planning area. The role of an area plan is to guide new development and redevelopment into patterns of orderly development, within an arrangement determined by the community of the planning area. To ensure that the goals and policies of this plan are considered during the development approval process, decision-makers are asked to consider the following criteria.



## Criteria Applicable to the Entire Planning Area

### **DOES THE REQUESTED APPROVAL OR ACTION ALLOW PLANNED AND PHASED GROWTH BASED ON SMART GROWTH PRINCIPLES? THESE INCLUDE:**

- Development outside of the development priority zone must pay for its own infrastructure.
- Preference is given to development approvals where infrastructure is already in place (for both infill and fringe/new development) to ensure the full use of infrastructure facilities and services.
- Infrastructure incentives, where provided, promote multiple planning goals and policies.

### **DOES THE REQUESTED DEVELOPMENT APPROVAL OR ACTION PROVIDE FOR LIVABLE COMMUNITIES THROUGHOUT THE PLANNING AREA? THESE INCLUDE:**

- A mix of uses in community centers and other locations where compatible.
- Development (residential and nonresidential) that is bike and pedestrian oriented.
- Street layout and design that reflects the functional areas (nodes, centers, residential, business/industrial) of the community.
- Development that is designed at a "human scale".
- The creation of or arrangement for public places.
- "Clean" development to ensure the future quality of the environment - air, water and land.
- Non-compatible uses that are separated by buffers, transitions and open spaces.

### **WILL THE PROPOSED DEVELOPMENT OR ACTION PROMOTE A VILLAGE-LIKE DESIGN AND ENVIRONMENT OF BOTH RESIDENTIAL AND COMMERCIAL AREAS? THIS SHOULD CONSIDER:**

- Convenient locations for goods and services to serve the local community (quality goods, restaurants and shopping).
- Residential neighborhoods that have amenities including parks/public areas, hiking trails and community services.
- Commercial centers that are linked visually and functionally to surrounding neighborhoods.
- Mixed-use centers that serve as the focal point for the surrounding community.
- Promote reduction in vehicle miles by making walking, bicycling and transit more appealing.

# IMPLEMENTATION

**IN THE AREAS AROUND INDUSTRIAL AREAS, IS DEVELOPMENT FOCUSED ON THE CREATION OF A MIXED-USE BUSINESS AND INDUSTRIAL PARK TO WORK IN CONJUNCTION WITH DEVELOPMENT ON THE SITE? THESE INCLUDE:**

- Efforts to attract “clean” business and industry to the area.
- Efforts to attract quality jobs to the area.
- Promotion of economic development to meet the above.
- Location of uses surrounding industrial area that are compatible with the Industrial Park and have potential for shared interaction between the community and the park.

**HOW DOES THE PROPOSED DEVELOPMENT OR ACTION EXPAND THE PARKS, RECREATION AND OPEN SPACE SYSTEM IN THE PLANNING AREA? THIS SHOULD CONSIDER:**

- Conservation of open space during development layout, planning and approval.
- Long-term development of a linked-system of trails and open space.
- Promotion of preservation of natural features as a community quality of life enhancement.

**WILL THE PROPOSED DEVELOPMENT OR ACTION WORK TO MAINTAIN A HEALTHY ENVIRONMENT WITH HIGH AIR AND WATER QUALITY AND HABITAT FOR WILDLIFE? IT SHOULD PROVIDE:**

- Protection of natural resources when designing and siting new development.
- Minimizing impacts on the environment when developing or redeveloping property.
- Protection of the Blue River corridor and its environs as a habitat for wildlife.

**WILL THE PROPOSED DEVELOPMENT OR ACTION IMPROVE THE MULTI-MODAL VEHICULAR TRANSPORTATION SYSTEM THROUGHOUT THE COMMUNITY? SPECIFIC CRITERIA INCLUDE:**

- Working with surrounding communities to improve highways, including M-150 and US Highway 71.
- Improving the arterial system, including Holmes Road, 135th Street, and others.
- Minimizing the impact of and conflicts with rail traffic.
- Ensuring adequate access for emergency services.
- Prohibiting over-the-road vehicles through residential neighborhoods.
- Considering multi-modal transportation options, including transit, automobile, bicycle and pedestrian routes.

## Criteria Applicable Within Industrial Park Areas

### HOW WILL THE PROPOSED DEVELOPMENT APPROVAL OR ACTION COMPLY WITH THE DEVELOPMENT OF MIXED-USE PLANNED INDUSTRIAL AND BUSINESS PARK AREAS? THIS WOULD INCLUDE:

- Provision of offices, research and development, and specialized manufacturing and distribution establishments, all of a non-nuisance type.
- Development of a clean and attractive work environment in a campus setting.

### WILL THE PROPOSED DEVELOPMENT APPROVAL OR ACTION SUPPORT THE DESIGN A DEVELOPMENT THAT IS COMPATIBLE WITH SURROUNDING USES AND NATURAL CONSTRAINTS? IT SHOULD CONSIDER:

- Mix or segregation of uses within these areas depending upon: external impact, transportation demand, hours of operation, light and noise, size of structure, infrastructure demand, and natural site constraints.
- The need for a mixed-use center within or adjacent to the perimeter to serve residents, employees and businesses.

### DOES THE DEVELOPMENT APPROVAL OR ACTION ADDRESS TRANSPORTATION ISSUES, SUCH AS:

- Consideration and mitigation of key transportation issues (on and off-site): impact of truck traffic on adjacent neighborhoods, park access, noise, long term traffic generation and capacity, and transportation management.
- Methods of management and mitigation, including zoning, restrictive covenants, and site design approval.

### DOES THE DEVELOPMENT APPROVAL OR ACTION PROVIDE AMENITIES? THESE INCLUDE:

- Retention and provision of open space, public gathering places, public facilities and services (police station, post office), recreation facilities, and limited retail sales and services.

### DOES THE DEVELOPMENT APPROVAL OR ACTION ENSURE BUILDING DESIGN AND PLACEMENT THAT IS QUALITY, DESIGNED FOR LONG-TERM DURABILITY AND POTENTIAL REUSE? THESE INCLUDE:

- Compliance with design guidelines.
- “Smart” and “sustainable” building architecture as generally recognized within the architectural profession.
- Provision of landscaping, buffering and screening.

### AS PART OF THE DEVELOPMENT APPROVAL OR ACTION, WILL THE FOLLOWING BE CONSIDERED?:

- Clear identification of allowable and prohibited uses.
- Specification and consideration of development impacts including: traffic (roadway and intersection capacity), environment (air and water quality impact), fiscal (infrastructure investment and tax policies).
- Establishment of performance guidelines for construction and operation of uses.

# IMPLEMENTATION

## Implementation Matrix

The implementation program of the Area Plan identifies the action steps to be taken to use this plan in a matrix included at the end of this section. As these steps are taken and private developments are proposed, decision-makers are asked to consider whether the actions will be in conformance with this plan by application of the following decision-making criteria.

The proposed land use, design guidelines, development concepts, and other recommendations in this plan may be implemented through the matrix action steps below. These steps are organized by FOCUS Building Block concepts with organizational responsibilities and time frames offered as part of the entire implementation strategy. A short time frame is 0 to 2 years. A middle range time frame is 2 to 5 years. A long term time frame is greater than 5 years. Some actions span across time frames, and thus are continuous or ongoing in nature.



Action Steps	Responsibility: ●Primary ○Secondary					Time Frame			
	City	Civic & Business Organizations	Residents	Developer	Partner-ships	Short Range	Mid-Range	Long-Range	On-going
<b>Quality Places to Live and Work</b>									
Create and enforce design guidelines for residential, commercial and business/ industrial parks that promote physical connections, a sense of place, and the protection of adjacent areas through directing the form and quality of development	●	○	○	○		◆			◆
Identify appropriate locations for new public and civic uses, particularly in mixed-use centers	●			○	○				◆
Consider the creation of a Pedestrian Oriented overlay district for the MCCID area and mixed-use centers proposed in this Plan	●	○	○	○		◆			
Establish streetscape and landscape plans for mixed-use community centers and corridors	●	○				◆			◆
Explore rezoning within the MCCID area to implement the Land Use Plan	●	○	○	○		◆			
Establish a plan for the timing of infrastructure provision and the availability of infrastructure capacity for new development consistent with the <i>FOCUS</i> Development Priority Zone	●	○	○	○					◆
Concentrate public capital investments in areas that are contiguous with currently developed land and that are currently or easily served by utilities	●			○					◆

Action Steps	Responsibility: ●Primary ○Secondary					Time Frame			
	City	Civic & Business Organizations	Residents	Developer	Partner-ships	Short Range	Mid-Range	Long-Range	On-going
Identify locations of future trails, paths and greenways and acquire or reserve these areas; establish requirements for their creation in the zoning ordinance	●		○	○	○	◆			
Determine the feasibility for increased public transportation serving the Martin City mixed-use center and Centerpoint Intermodal Facility	●	○			○		◆		
Adopt policies that prohibit or restrict through truck traffic in residential areas	●				○	◆			
Complete the streetscape design concepts for the 135th Street Corridor in Martin City	●	○					◆		
Construct the 135th Street streetscape improvement in Martin City	●	○					◆		
Conduct a Safe Routes to School Plan for the Martin City Elementary School area	●	○	○			◆			
Establish buffering and screening requirements and transitional uses or spaces to separate dissimilar development types	●	○	○	○		◆			
<b>Neighborhood Livability</b>									
Assist local organizations with Public Improvement Advisory Committee (PIAC) application process and use Community Infrastructure Committee (CIC) Report criteria to prioritize capital improvements	○	●	●						◆

Action Steps	Responsibility: ●Primary ○Secondary					Time Frame			
	City	Civic & Business Organizations	Residents	Developer	Partner-ships	Short-Range	Mid-Range	Long-Range	On-going
Review existing zoning and encourage property owners to rezone or downzone in "open" zoning districts where zoning and land use are not in conformance	●	○	○	○					◆
Explore funding mechanisms available to retrofit existing neighborhoods to include sidewalks and streetlights	○	○	○		●		◆		
<b>City Life</b>									
Create public use focal points in mixed-use centers	○	●		●					◆
Construct "gateways" into Martin City to better identify the entrances to the area	●	○	○				◆		
Create a gateway/internal marker program for new Industrial and Business Park	○			●			◆		
Target incentives to encourage public/private partnerships in mixed-use areas for public improvements	●	○	○		●				◆
<b>FOCUS Centers and Community Development Catalysts</b>									
Develop Martin City as a mixed-use/ transportation center as defined in <i>FOCUS</i>	●	○		○	●			◆	
<b>Competitive Economy</b>									
Educate business owners in Martin City about special incentives and programs such as the Façade Rebate Program	●	○					◆		
Promote neighborhood scale business investment in areas outside of the planned industrial and business parks.	○	●		●	●				◆

Action Steps	Responsibility: ●Primary ○Secondary					Time Frame			
	City	Civic & Business Organizations	Residents	Developer	Partnerships	Short Range	Mid-Range	Long-Range	On-going
Coordinate public investments to support existing business areas and development programs	●	○		○					◆
Determine the feasibility of extending transit service to support employment and mixed-use centers in the area	○	○			●		◆		
Study the feasibility, timing, cost, and responsibility of utility extensions into both industrial areas and the southwestern portions of the planning area	●			●		◆			
Prioritize public improvements required to implement this plan and incorporate into the City's CIP	●					◆			
<b>Connecting Corridors and Moving About the City</b>									
Create and encourage pedestrian amenities throughout the planning area	●	○	○	○					◆
Undertake comprehensive traffic studies and develop access control plans for major existing and emerging corridors	●	○		○		◆			
Target development incentives to reinforce mixed-use and/or transit oriented corridors	●			○	○				◆
Encourage corridor development in "nodes" rather than strips throughout the planning area and particularly along arterials	●			○					◆



Action Steps	Responsibility: ●Primary ○Secondary					Time Frame			
	City	Civic & Business Organizations	Residents	Developer	Partner-ships	Short Range	Mid-Range	Long-Range	On-going
Enhance the recreation corridor along the Big Blue River through greenway preservation, creation of bike and pedestrian paths, and links to neighborhood green and open spaces, as well as the extension of greenways into the area as development occurs	●	○	○		○				◆
Replace the Kenneth Road bridge over the Blue River	●					◆			
Complete the proposed Holmes Road improvements north of the planning area	●					◆			
Amend the City's Major Street Plan to include the recommendations of this plan, including proposed arterial and bridge improvements	●					◆			
Update the Holmes Road Improvement Study and re-evaluate the need for a grade separated crossing of the Union Pacific Rail Line in the MCCID.	●	○	○			◆			
Study the feasibility of further rail/automobile grade separations as needed	●				●				◆
<b>Citizen Access and Communication</b>									
Establish a plan of communication committee from local residents that meets regularly with City staff to communicate about neighborhood and area issues	●		○						◆

# MARKET ANALYSIS

## MCCID Market Analysis (Abbreviated Summary)

NOTE: See the full market report and the PowerPoint for additional text and possible graphics to insert into the Area Plan as appropriate to fill the space on the page layout.

In March 2008 an economic market analysis was conducted for the Martin City CID and the surrounding trade area. The analysis specifically addressed Martin City retail market potential, targeted business types suitable for the MCCID area, and potential development of new housing units in the MCCID and immediate surrounding area.

### RETAIL MARKET POTENTIAL

Given the configuration of commercial land, vacant commercial buildings and/or retail space in Martin City CID, the design of new in-fill buildings on 135th Street and retail industry averages, there is a potential for approximately 120,000 additional square feet of retail in approximately 26 to 32 additional businesses. The number, mix of businesses and estimated square footages that can be absorbed by Martin City CID in the future are presented in the table below.

Table xxxx - Martin City Projected Retail Summary by Use

Type of Retail Business	Potential/Recommended Number of Establishments	Estimated Sq. Ft.
Full service Restaurants	3 – 4	13,500
Sandwich / Fast food	1	1,500
Retail Shops	12 – 15	48,000
Arts (i.e. community theater) / Private Recreation i.e. exercise/fitness facility/athletic club, greenway/trail related businesses)	3 – 4	40,000
Retail Services	7 – 8	17,000
<b>Total</b>	<b>26 – 32</b>	<b>120,000</b>

Source: Richard Caplan and Associates, Urban Land Institute Dollars & Cents of Shopping Centers.

## **HOUSING MARKET POTENTIAL**

To successfully achieve Martin City's residential potential, the housing market will be influenced by the following dynamics and demographics:

1. The growth of the employment base in the immediate area especially the 1,444 acre Center Point redevelopment project serving the new intermodal facility at former Richards-Gebaur Air Force Base;
2. The adoption and implementation of a Martin City Master Plan that includes improvements to 135th Street that incorporate curbs, sidewalks, decorative street lighting and attractive landscaping resulting in 135th Street being an attractive amenity to the CID area;
3. Continued marketing of Martin City in the region as a unique commercial district within the metropolitan area;
4. Potential improvements and/or expansion to the Martin City School by the school district; and
5. Further strengthening the commercial appeal of Martin City businesses through private reinvestment in the exterior landscaping and building design, product lines and customer services offered.

Based on the demographic characteristics of the existing and projected population, housing unit mix and the residential conditions in the Martin City CID area can support:

- Approximately 300-325 new housing units
- Single-family detached (50-55% recommended mix)
- Single-family attached-townhomes-patio homes (19-21% recommended mix)
- Multifamily units in mixed use buildings and other multifamily developments (25-30% recommended mix)